

ATTACHMENT 3
Transport Impact Statement

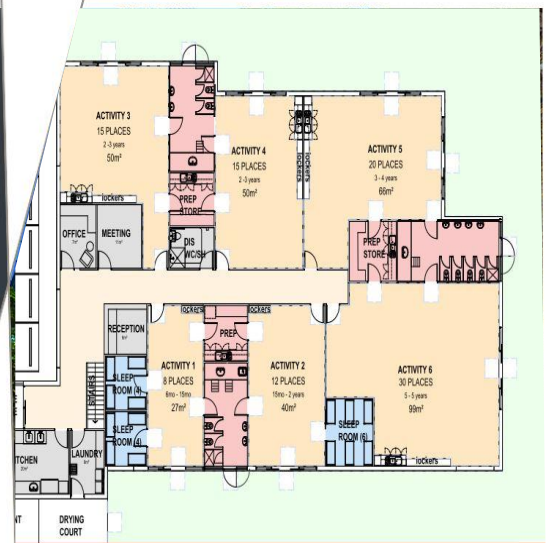
Transport Impact Statement

Proposed Child Care Centre – Lot
119 (No. 390) Kingsway, Lansdale

CW1157000

Prepared for
LP WA No.1 Pty Ltd

13 August 2021



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Document Information

Prepared for	LP WA No.1 Pty Ltd
Project Name	Proposed Child Care Centre – Lot 119 (No. 390) Kingsway, Lansdale
File Reference	CW1157000-TR-RT-001- TIS-A-No. 390 Kingsway, Lansdale
Job Reference	CW1157000
Date	13 August 2021
Version Number	C

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Effective Date 13/08/2021

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Date Approved 13/08/2021

Document History

Version	Effective Date	Description of Revision	Prepared by	Reviewed by
A	21/06/2021	For Issue	DR	RJC
B	11/08/2021	Minor update	BS	SJL
C	13/08/2021	For Issue	BS	SJL

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1 Introduction

1.1 Background

Cardno was commissioned by LP WA No. 1 Pty Ltd (“the Client”) to prepare a Transport Impact Statement (TIS) for a proposed Child Care Centre located at No. 390 Kingsway, Lansdale.

This TIS has been prepared in accordance with the *Western Australian Planning Commission (WAPC) Transport Impact Assessment Guidelines for Developments: Volume 4 – Individual Developments (2016)* and the checklist is included in **Appendix A**.

1.2 Site Location

The Site is located at No. 390 Kingsway, Lansdale. **Figure 1-1** shows an aerial image of the Site. It is currently occupied by a single residential dwelling. Access is provided via a single crossover on Kingsway.

Figure 1-1 Aerial Image of Site



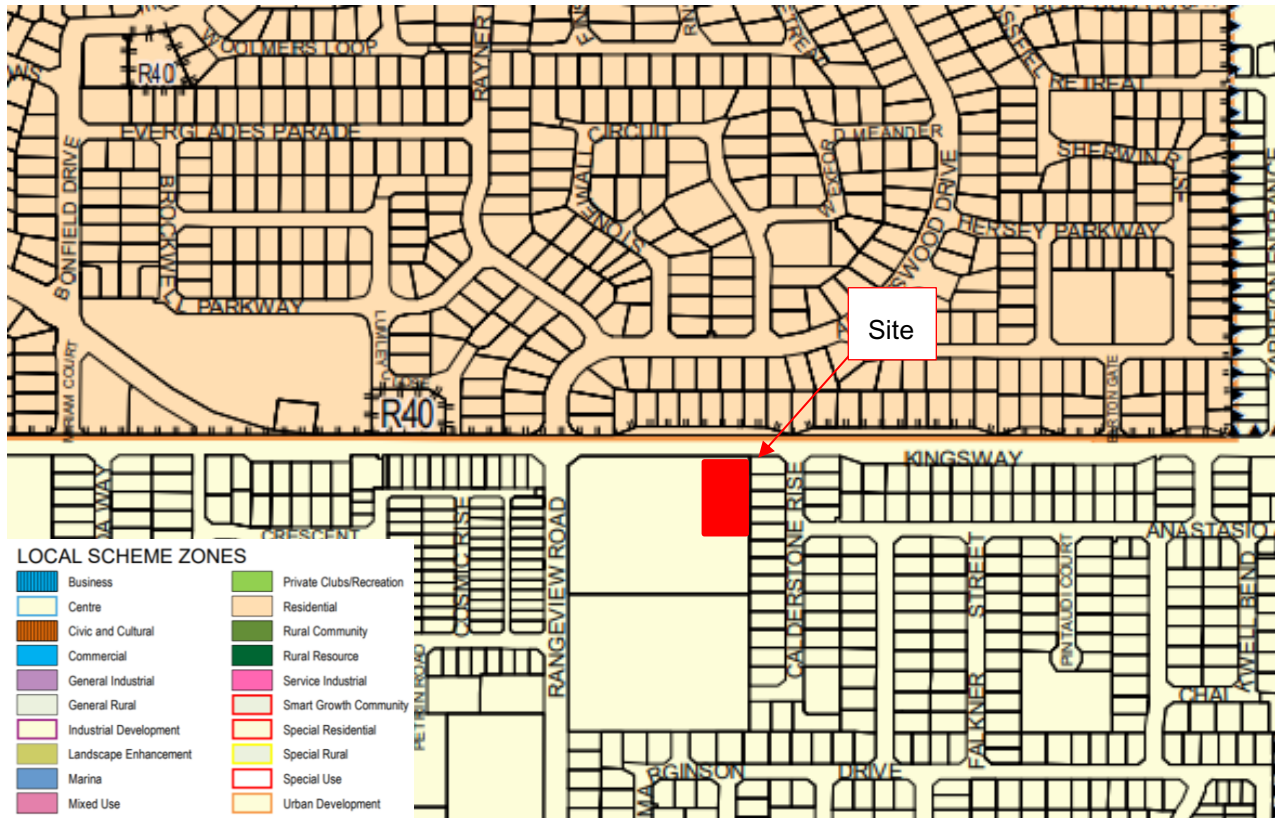
Source: Nearmap (2021)

1.3 Surrounding Land Uses

Pursuant to the provision of the *City of Wanneroo District Planning Scheme No. 2 (DPS2)*, the Site is zoned “Urban Development” as shown in **Figure 1-2**.

The Site is surrounded by Residential land uses to the north and other Urban Development land uses to the east, south and west.

Figure 1-2 City of Wanneroo Zoning



Source: *City of Wanneroo District Planning Scheme No. 2*

1.4 Existing Road Network

Road classifications are defined in the Main Roads Functional Hierarchy as follows:

- > **Primary Distributors (light blue):** Form the regional and inter-regional grid of MRWA traffic routes and carry large volumes of fast-moving traffic. Some are strategic freight routes, and all are National or State Roads WA.
- > **Regional Distributors (red):** Roads that are not Primary Distributors, but which link significant destinations and are designed for efficient movement of people and goods within and beyond regional areas. They are managed by Local Government
- > **District Distributor A (green):** These carry traffic between industrial, commercial and residential areas and connect to Primary Distributors. These are likely to be truck routes and provide only limited access to adjoining properties. They are managed by Local Government.
- > **Distributor B (dark blue):** perform a similar function to District Distributor A but with reduced capacity due to flow restrictions from access to and roadside parking alongside adjoining property. These are often older roads with traffic demand in excess of that originally intended. District Distributor A and B roads run between land-use cells and not through them, forming a grid that would ideally be around 1.5 kilometres apart. They are managed by Local Government.
- > **Local Distributors (orange):** Carry traffic within a cell and link District Distributors at the boundary to access roads. The route of the Local Distributor discourages through traffic so that the cell formed by the grid of District Distributors only carries traffic belonging to or serving the area. These roads should accommodate buses but discourage trucks. They are managed by Local Government.

1.5 Existing Traffic Volumes

The most recent traffic volume in the vicinity of the Site were extracted from Main Roads Traffic Map and Road Information Mapping System and are shown in **Table 1-2**. The existing two-way daily volume of 3,412vpd is well under the desirable capacity of a Local Distributor Road (6,000vpd).

Table 1-2 Daily Traffic Volumes

Location	Year	AM Peak Volume	PM Peak Volume	Daily Volume
Kingsway (West of Alexander Dr) Eastbound	2018	204 (7:45 AM)	112 (4:00 PM)	1,751
Kingsway West of Alexander Dr) Westbound	2018	79 (11:45 AM)	230 (5:15 PM)	1,661

1.6 Crash Assessment

A crash assessment for the surrounding road network of the Site has been completed using the Main Roads WA Reporting Centre, as shown in **Table 1-3**, **Table 1-4** and **Table 1-5**. The assessment covers all the recorded accidents for the 5-year period between 1 January 2016 to 31 December 2020.

Table 1-3 Total Crashes

TOTAL CRASHES						
Type of Crash (RUM Code)	Fatal	Hospital	Medical	Major Property Damage	Minor Property Damage	Total Crashes
Right Turn Thru	-	-	-	1	-	1
Rear End	-	-	-	1	1	2
Hit Object	-	-	-	-	2	2
Right Angle	-	-	-	1	-	1
Total	-	-	-	3	3	6

Table 1-4 Intersection Crashes

INTERSECTION CRASHES						
Intersection Name	Fatal	Hospital	Medical	Major Property Damage	Minor Property Damage	Total Crashes
Kingsway - Abbotswood Dr & Rangeview Rd	-	-	-	1	1	2
Kingsway - Calderstone Rise	-	-	-	-	1	1
Rangeview Rd - Kingsway & Abbotswood Dr	-	-	-	1	1	2
Calderstone Rise - Kingsway	-	-	-	-	1	1
Calderstone Rise - Anastasio Av	-	-	-	1	1	2
Total	-	-	-	3	5	8

Table 1-5 Midblock Crashes

MIDBLOCK CRASHES						
Road Name	Fatal	Hospital	Medical	Major Property Damage	Minor Property Damage	Total Crashes
Rangeview Rd	-	-	-	1	-	1
Total	-	-	-	1	-	1

Figure 1-4 shows the crash locations and their intensity along Kingsway, Rangeview Road and Calderstone Rise.

Figure 1-4 Crash Locations



A summary of the crash data is as follows:

- > There were no fatal accidents or crashes recorded requiring medical attention; and
- > All crashes recorded resulted in major/minor property damage.

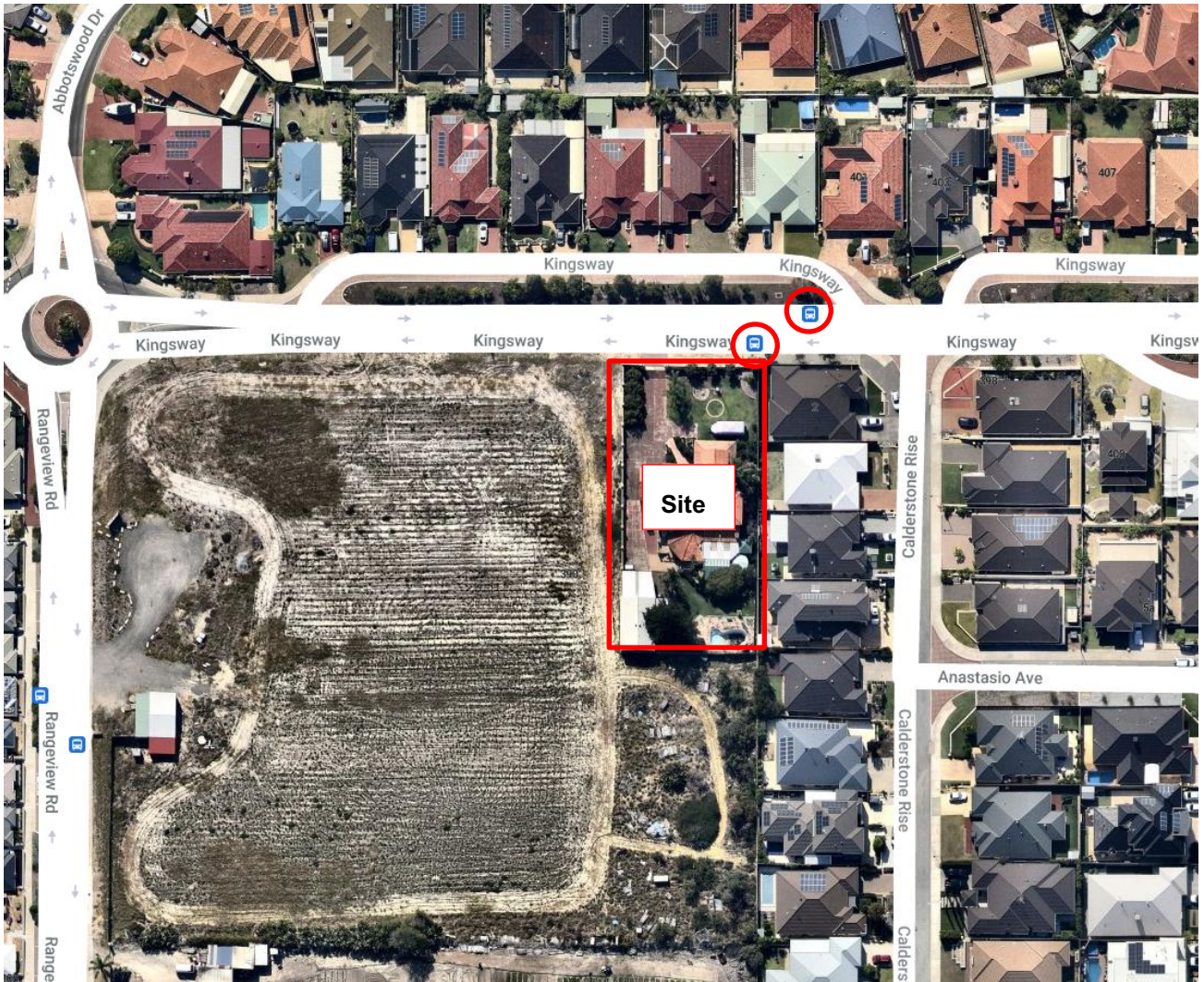
It is very unlikely that this development would have any material impact on the road safety in the area due to its small scale.

2 Public Transport Facilities

2.1 Existing Public Transport Facilities

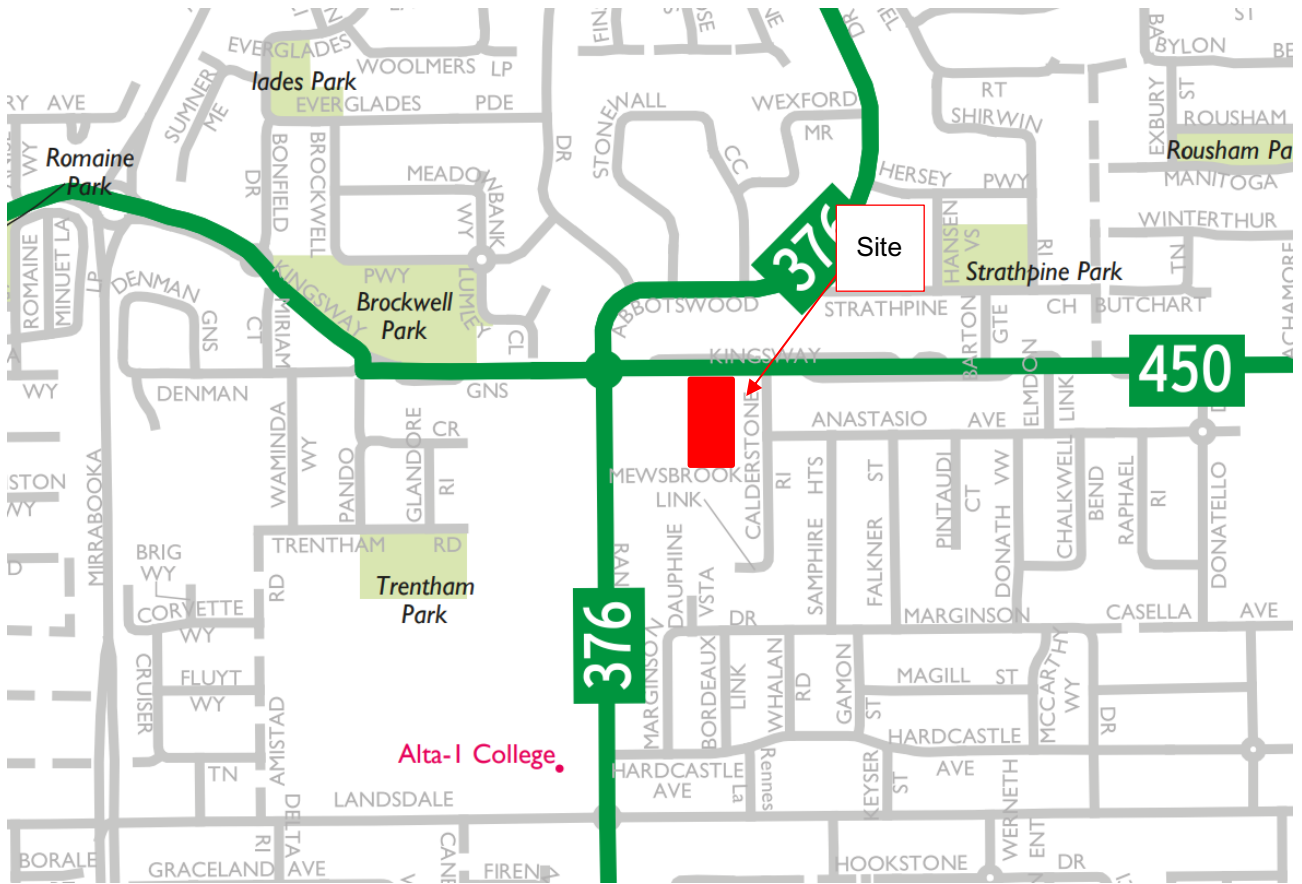
The nearest bus stops are located in-front of the Site, as shown in **Figure 2-1**. Bus route 450 operates from this stop along Kingsway and travels to Warwick Station, as shown in **Figure 2-2**. The service runs approximately every 10-15 minutes in the AM peak towards Warwick Station and every 15 minutes in the PM peak towards Lansdale.

Figure 2-1 Nearest Bus Stops



Source: Nearmap (2021)

Figure 2-2 Existing Bus Routes



Source: Transperth Network Maps (2021)

2.2 Future Public Transport Facilities

Cardno contacted the Public Transport Authority and understand there are no proposed changes to the public transport network in this area.

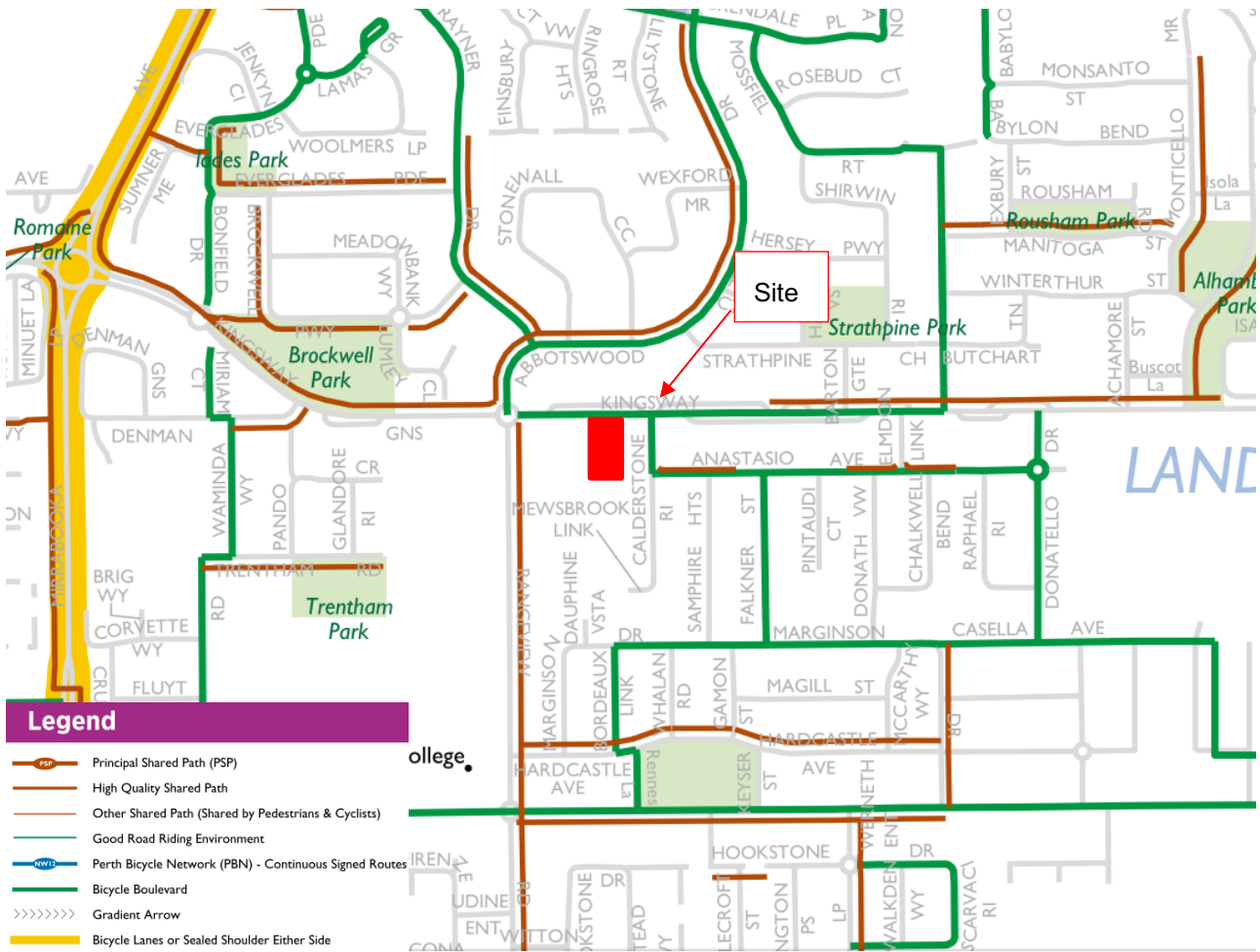
3 Pedestrian/Cycle Networks and Facilities

3.1 Existing Pedestrian/Cycle Network Facilities

High quality shared paths run along Rangeview Road and sections of Kingsway and Anastasio Avenue, as shown in **Figure 3-1**. “Good Road Riding Environments” stretch along portions of Kingsway, Calderstone Rise and Anastasio Avenue.

A footpath is provided along Kingsway. Overall, pedestrian/cycle access to the Site is good.

Figure 3-1 Pedestrian and Cycle Network



Source: Department of Transport Cycling Network Maps

3.2 Future Pedestrian/Cycle Network Facilities

Cardno contacted the City of Wanneroo and understand there are no proposed changes to the pedestrian and cycling network to the vicinity of the Site in the near future.

4 Proposed Development

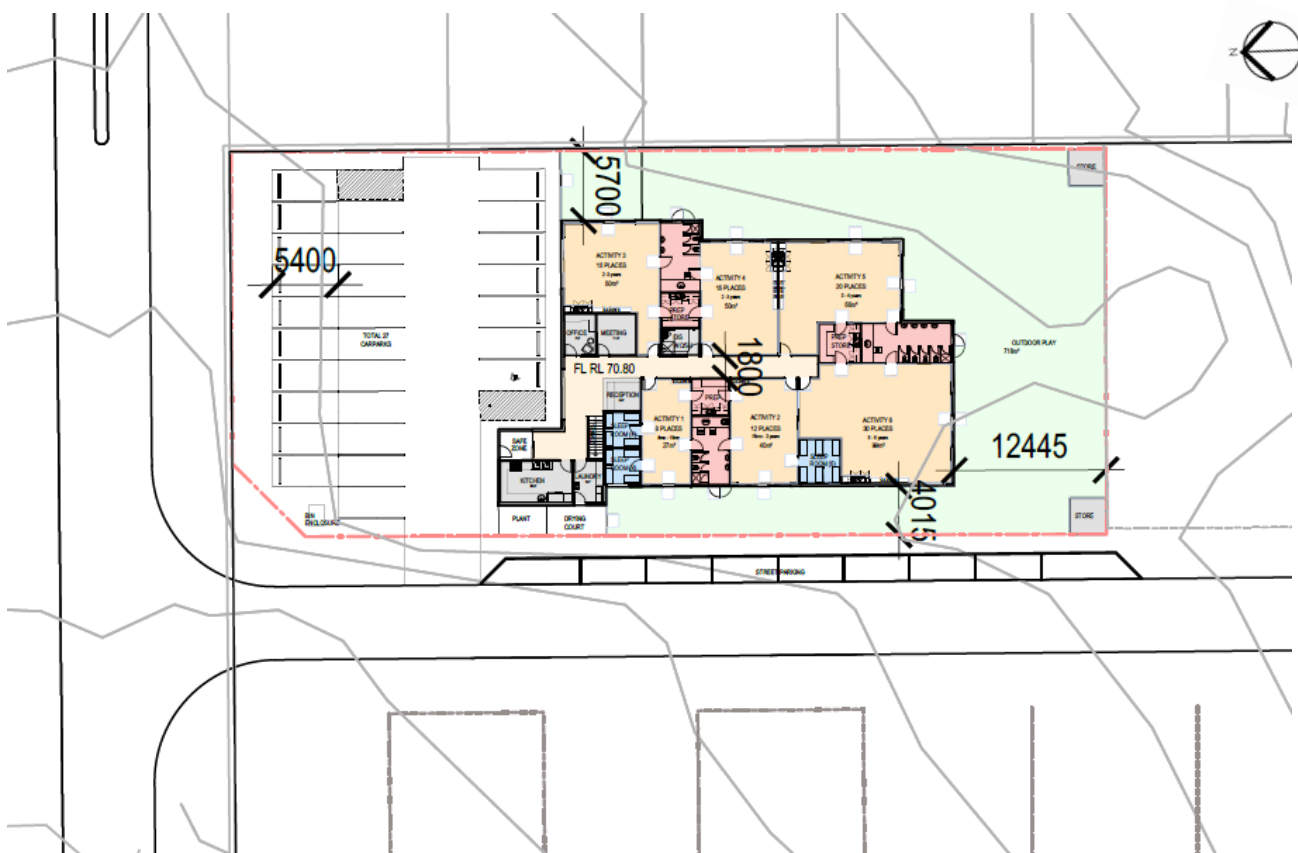
4.1 Proposed Development

The proposal is for a child care centre, comprising of the following:

- > 100 children;
- > 16 staff members; and
- > 27 car parking bays (including 1 ACROD Bay) on site.
- > Up to 10 on street parking bays within adjacent verge area.

The layout of the proposed childcare at the Site is shown below in **Figure 4-1**, with larger versions included in **Appendix B**.

Figure 4-1 Ground Floor Plan



Source: On Architecture (2021)

4.2 Operating Hours

The Child Care Centre is proposed to operate during the following days and times:

- > Monday to Friday (6:00AM-7:00PM).

4.3 Waste Collection

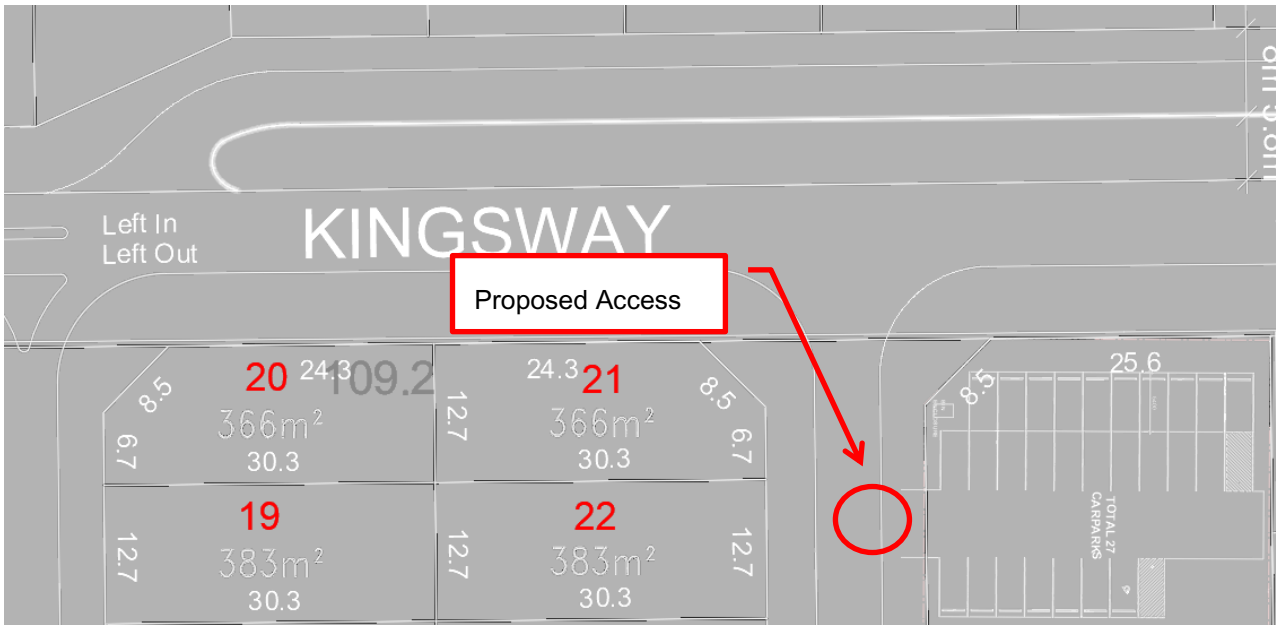
The waste collection of the development is proposed to be carried out as per the existing bin collection procedure. Bins will be wheeled out on bin collection days along the verge of Kingsway or the local road to be constructed along the site's western boundary, emptied, and returned to the Site.

4.4 Access Arrangements

As shown in **Figure 4-2**, the proposed site access is located on the north western edge of the site, via the proposed internally road network within the adjacent future subdivision, connected to Kingsway Road. The proposed access road exceeds the minimum 20m separation from either access point to the CAP road located within the northern verge of Kingsway Road and the location of the carpark access is positioned to exceed the driveway to intersection separation requirements as stipulated within AS/NZ2890.1

The conceptual layout of the proposed subdivision road network and the access of the proposed development is shown in **Figure 4-2**.

Figure 4-2 Site Access



Source: On Architecture (2021)

4.5 Traffic Generation

Trip generation rates from the *RTA Guide to Traffic Generating Development* were used to estimate the number of vehicle trips generated by the Site. The trip generation rate, distribution and development trip generation for the proposed child care centre is summarised in **Table 4-1**, **Table 4-2** and **Table 4-3**.

Table 4-1 Adopted Trip Generation Rates

Land Use	ITE Code/Source	AM Peak	PM Peak
Child Care Centre	RTA V2.2	0.80 trip per child	0.70 trip per child

Table 4-2 Trip Directionality Rates

Land Use	Yield	ITE Code/Source	AM Peak		PM Peak	
			In	Out	In	Out
Child Care Centre	100 children	RTA V2.2	53%	47%	47%	53%

Table 4-3 Estimated Trip Generation

Land Use	Yield	ITE Code/Source	AM Peak		PM Peak	
			In	Out	In	Out
Child Care Centre	100 children	RTA V2.2	43	38	33	37
Total			81		70	

The calculated trip generation of the proposed development is 81 trips in the AM Peak and 70 trips in the PM Peak. The traffic generated daily is approximately 4% of the existing daily volume, which would still ensure the total volume along Kingsway stays under the desirable volume. Hence, the trip generation is considerably low and is not expected to result in any material impact on the surrounding road network.

4.6 Swept Paths

A B99 design vehicle has been used in assessing the manoeuvrability of the proposed subdivision road and child care centre access. The swept paths shown in **Figure 4-3** indicates that the proposed road layout and access arrangement can accommodate a B99 vehicle swept path.

Figure 4-3 B99 Swept Path



5 Parking Supply

5.1 Parking Requirements

The statutory parking requirements, in accordance with the *City of Wanneroo Local Planning Policy 2.3 – Child Care Centres*, have been considered in the context of the proposed development and are summarised below in **Table 5-1**.

Table 5-1 Car Parking Provision and Requirements

Development Classification	Requirements	Yield	Parking Required	Parking Provided
Child Care Centre	1 bay per staff member, plus for 55 or more children to provide 9 bays plus 1 per 8 children accommodated in excess of 54	16 staff members 100 children	31 bays	27 bays

27 car parking bays are proposed on-site. Whilst this identifies a minor 4 bay shortfall, the location of the Site aims to predominately service the nearby local community, adjacent to a frequent bus service. As such, it is anticipated that there would be a portion of parents/caretakers that would walk to the centre drop-off children and then use public transport. The easy access to bus stops would also be attractive to employees, thus potentially freeing up additional parking supply to parents.

Further to the onsite parking, up to an additional 10 on-street bays are proposed to be constructed within the adjacent verge of the future road reserve located to the immediate west of the child care, which could be used to supplement the parking bays shortfall of the proposed development.

Overall, the proposed parking supply is considered appropriate to accommodate the Sites operational demand and should be supported.

With regard to the provision and usage of tandem parking bays, it is proposed that the carpark will be managed so as to ensure that staff of the centre park at the far ends of the tandem bays to ensure that the maximum number of aisle accessible bays are available for parent pick-up drop off use.

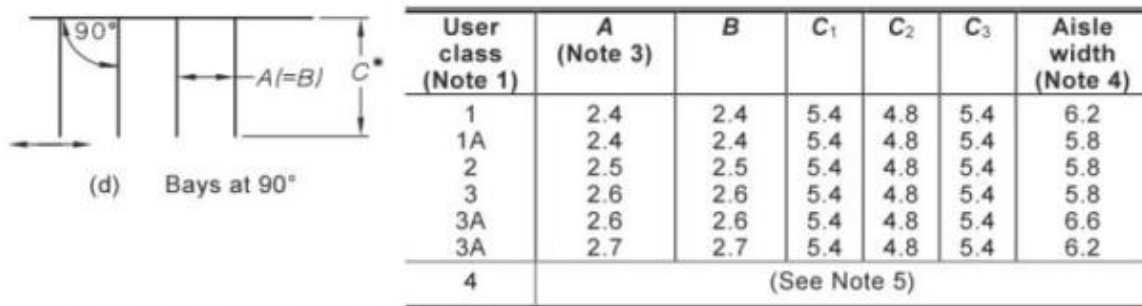
5.2 Parking Compliance with Australian Standards

The Site’s parking bay location and layout appears to be in accordance with the Australian Standard AS2890.1:2004 – Part 1: Off-street Car Parking. The bay geometry and layout dimensions are to be consistent with the following user class:

- > Child Care Centre – User Class 3 (short-term city and town centre parking, parking stations, hospitals and medical centres)

Figure 5-1 shows the parking specifications for the given user class.

Figure 5-1 Car Parking Dimension Specifications



It is noted that AS2890.1 does not provide any specific guidance for tandem bay requirements, which leaves the appropriate dimensions to be interpreted by the Local Government approval body. The City of Wanneroo does not provide guidance as to tandem bays in its parking policy. Cardno has undertaken research to ascertain the position of other Local Government agencies. It is discovered that other local governments would generally identify 10m as the minimum for tandem bays.

The length of the proposed tandem bays is 10.8m, which is deemed acceptable.

A turn around bay (marked as “keep clear”) is also to be provided within the car park to assist in vehicles leaving the carpark in a forward gear should the carpark be momentarily full.

5.3 Bicycle Parking Requirements

Both the *City of Wanneroo District Planning Scheme No.2* and *Local Planning Policy 2.3 – Child Care Centres* do not outline requirements for the provision of bicycle parking facilities for the proposed land use at the Site.

6 Summary

This Transport Impact Statement outlines the transport aspects of the proposed development focusing on traffic operations, access and provision of car parking. Included are discussions regarding pedestrian, cycle and public transport considerations.

This statement has been prepared in accordance with the *WAPC Transport Impact Assessment Guidelines for Developments: Volume 4 – Individual Developments (2016)*.

The following conclusions are evident about the proposal:

- > The proposed development is a Child Care Centre, accommodating up to 100 children and 16 staff;
- > The Site is located directly adjacent to a bus stop which is serviced by route 450;
- > The proposed development is expected to generate approximately 81 vehicle trips in the AM Peak and 70 vehicle trips in the PM Peak. This level of traffic generation is anticipated to have no material impact on the surrounding road network;
- > 27 car parking bays are proposed on-site. While there is a onsite parking shortfall of 4 bays from a statutory standpoint, up to 10 on street parking bays are also proposed to be provided and it is not expected that this would be detrimental to the site operation. Parking demand will also be mitigated to a certain degree by the site's location within a residential area and easy access to frequent bus services. Some parents are likely to combine their walking trip to the bus stop with dropping off their child at the centre, thus not needing a parking bay; and
- > Overall, the proposed development is unlikely to result in any material impact to the surrounding road network.

Proposed Child Care Centre – Lot 119
(No. 390) Kingsway, Lansdale

APPENDIX

A

WAPC CHECKLIST

Item	Status	Comments/Proposals
Proposed subdivision		
proposed land use	Section 4	
existing land uses	Section 1	
context with surrounds	Section 1	
Vehicular access and parking		
access arrangements	Section 4	
public, private, disabled parking set down / pick up	Section 2	
Service vehicles (non-residential)		
access arrangements	Section 4	
on/off-site loading facilities	N/A	
Service vehicles (residential)		
Rubbish collection and emergency vehicle access	Section 4	
Hours of operation (non-residential only)		
Traffic volumes		
daily or peak traffic volumes	Section 1	
type of vehicles (e.g. cars, trucks)	Section 1	
Traffic management on frontage streets		
Public transport access		
nearest bus/train routes	Section 2	
nearest bus stops/train stations	Section 2	
pedestrian/cycle links to bus stops/train station	Section 3	
Pedestrian access/facilities		
existing pedestrian facilities within the development (if any)	Section 3	
proposed pedestrian facilities within development	Section 3	
existing pedestrian facilities on surrounding roads	Section 3	
proposals to improve pedestrian access	NA	
Cycle access/facilities		
existing cycle facilities within the development (if any)	Section 3	
proposed cycle facilities within the development	Section 5	
existing cycle facilities on surrounding roads	Section 3	
proposals to improve cycle access	N/A	
Site specific issues		
N/A		
Safety issues		
identify issues	Section 1 & 4	
remedial measures	N/A	

Proposed Child Care Centre – Lot 119
(No. 390) Kingsway, Lansdale

APPENDIX

B

SITE PLANS

PRELIMINARY

ARCH DRAWING SCHEDULE

No.	Sheet Name
DA01	FRONT COVER DRAWING SCHEDULE
DA02	SITE PLAN
DA03	SUBDIVISION SITE PLAN
DA04	PROPOSED FLOOR PLAN
DA05	PROPOSED ROOF PLAN
DA06	LANDSCAPING PLAN
DA07	PROPOSED ELEVATIONS
DA08	FENCE ELEVATIONS
DA09	SITE SECTIONS
DA10	MATERIALS SELECTIONS
DA11	3D CONCEPT IMAGES

Rev	Amendment	Date
-	-	-



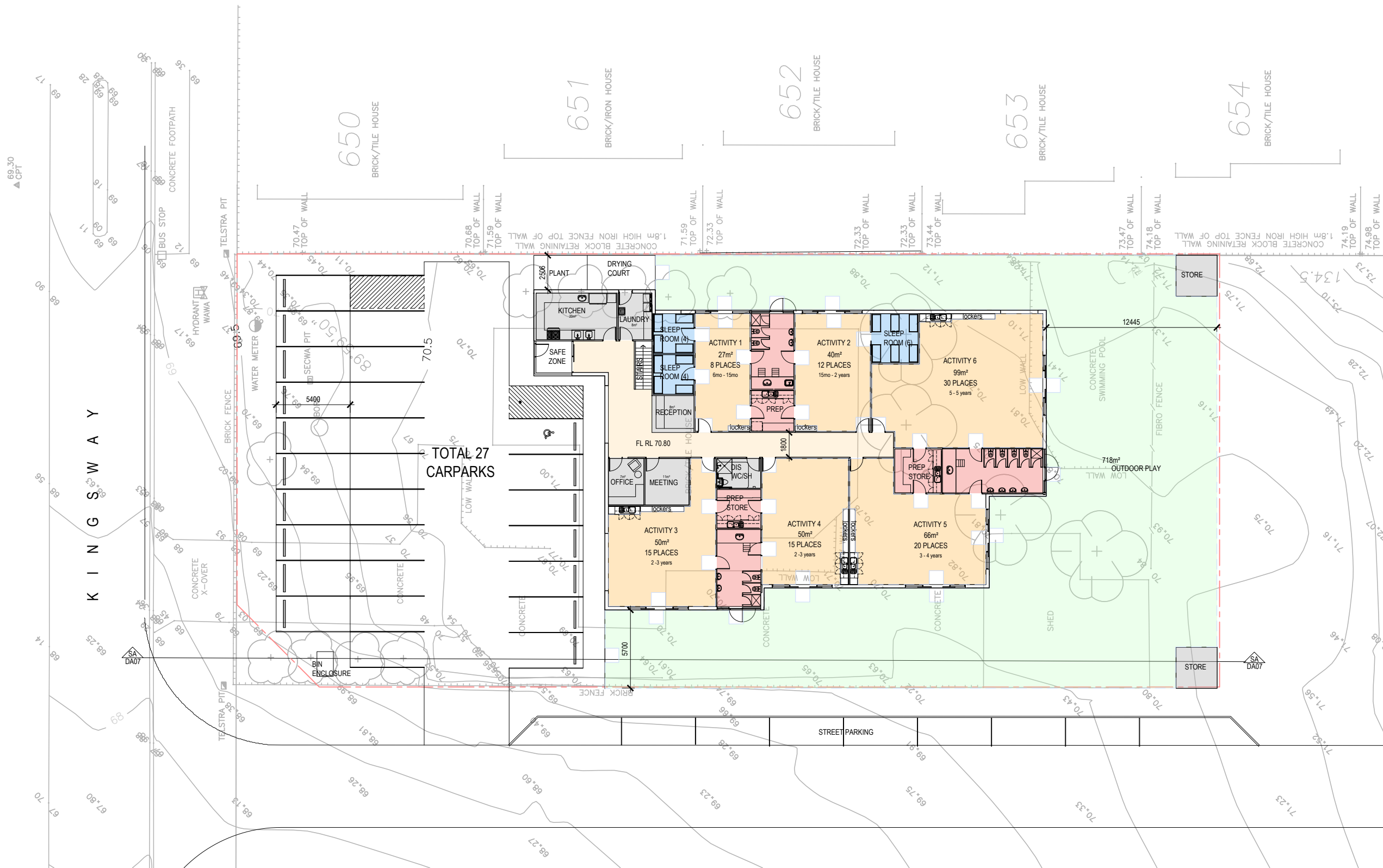
Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
DRAWING SCHEDULE

Scale As indicated Drawn
Client LEYTON PROPERTY
Date 04/08/2021
Job No. 202100033
Dwg No. **DA01**

Rev: - A3 SHEET





SITE PLAN
1:300



Project
100 PLACE CHILD CARE FACILITY
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Drawing
SITE PLAN

Scale As indicated Drawn MN
 Client
 Date 04/08/2021
 Job No. 202100031
 Dwg No. DA02 Rev: - A3 SHEET





SUBDIVISION SITE PLAN
1:1000



Project
100 PLACE CHILD CARE FACILITY
 Kingsway, Landsdale WA

Drawing
 SUBDIVISION SITE PLAN

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 Client LEYTON PROPERTY
 Date 23/06/2021
 Job No. 202100033
 Dwg No. DA03



Rev: A3 SHEET



PRELIMINARY

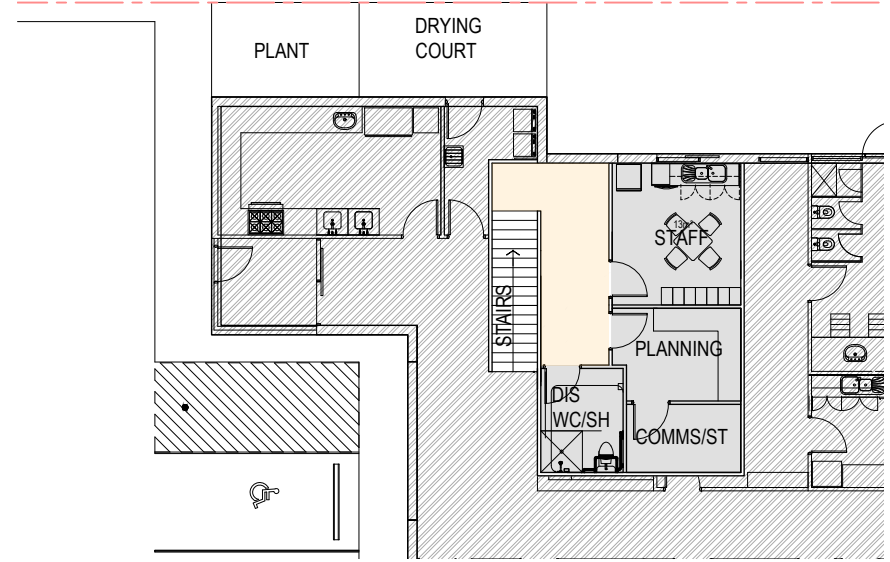
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PROPOSED FLOOR PLAN
1:200

OUTDOOR PLAY AREA SCHEDULE					
ROOMS	PLACES	AREA REQ	UNENCUMBERED AREA	ENCUMBERED AREA	TOTAL AREA PROVIDED
ROOM 1	8				
ROOM 2	12				
ROOM 3	15				
ROOM 4	15				
ROOM 5	20				
ROOM 6	30				
TOTALS	100	700	780sqm	12sqm	792sqm

SITE DATA	
SITE AREA	2249sqm
BUILDING AREAS (GFA)	681sqm
• Ground	52sqm
• First Floor	733sqm
TOTAL	733sqm
PROPOSED CHILD CARE PLACES	100
REQUIRED CARPARKING	1 bay per staff member, plus 9 bays when the proposed number of children is over 54, plus 1 bay per 8 children accommodated in excess of 54 31 spaces required
PROPOSED CARPARKING	28 Carparks

ACTIVITY AREA SCHEDULE									
ROOM	PLACES	AGE	STAFF RATIO	STAFF No.	AREA REQ	UNENCUMBERED AREA	ENCUMBERED AREA	TOTAL AREA PROVIDED	
ROOM 1	8	0-2	1:4	2	26	27sqm	7sqm	34sqm	
ROOM 2	12	0-2	1:4	3	39	40sqm	7sqm	47sqm	
ROOM 3	15	2-3	1:5	3	49	50sqm	7sqm	57sqm	
ROOM 4	15	2-3	1:5	3	49	50sqm	7sqm	57sqm	
ROOM 5	20	3-4	1:10	2	65	66sqm	7sqm	73sqm	
ROOM 6	30	4-5	1:10	3	98	99sqm	7sqm	106sqm	
TOTALS	100			16	326	332sqm	42sqm	374sqm	



FIRST FLOOR PLAN
1:200

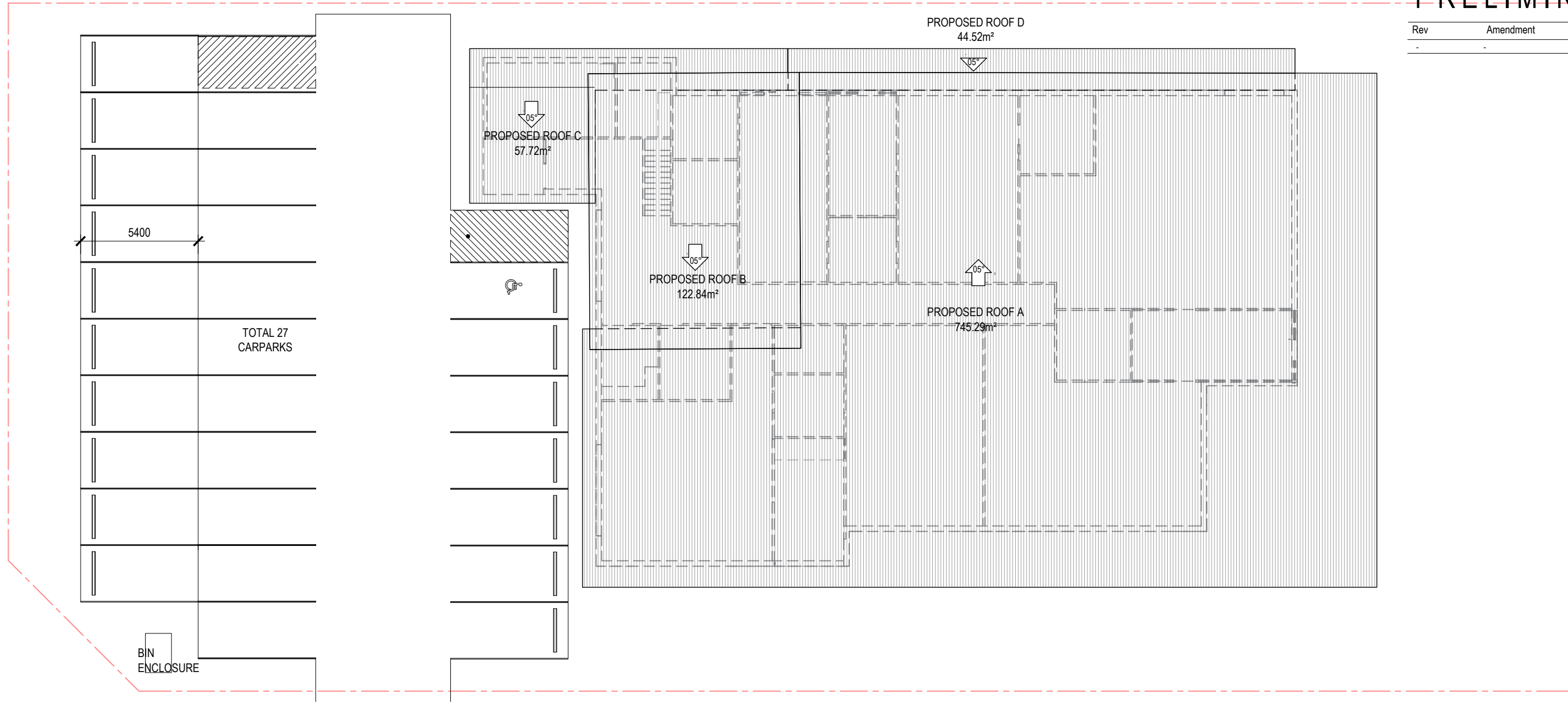


Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
PROPOSED FLOOR PLAN

Scale As indicated Drawn MN
Client LEYTON PROPERTY
Date 05/08/2021
Job No. 202100033
Dwg No. **DA04** Rev: - A3 SHEET

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PROPOSED ROOF PLAN
1:200



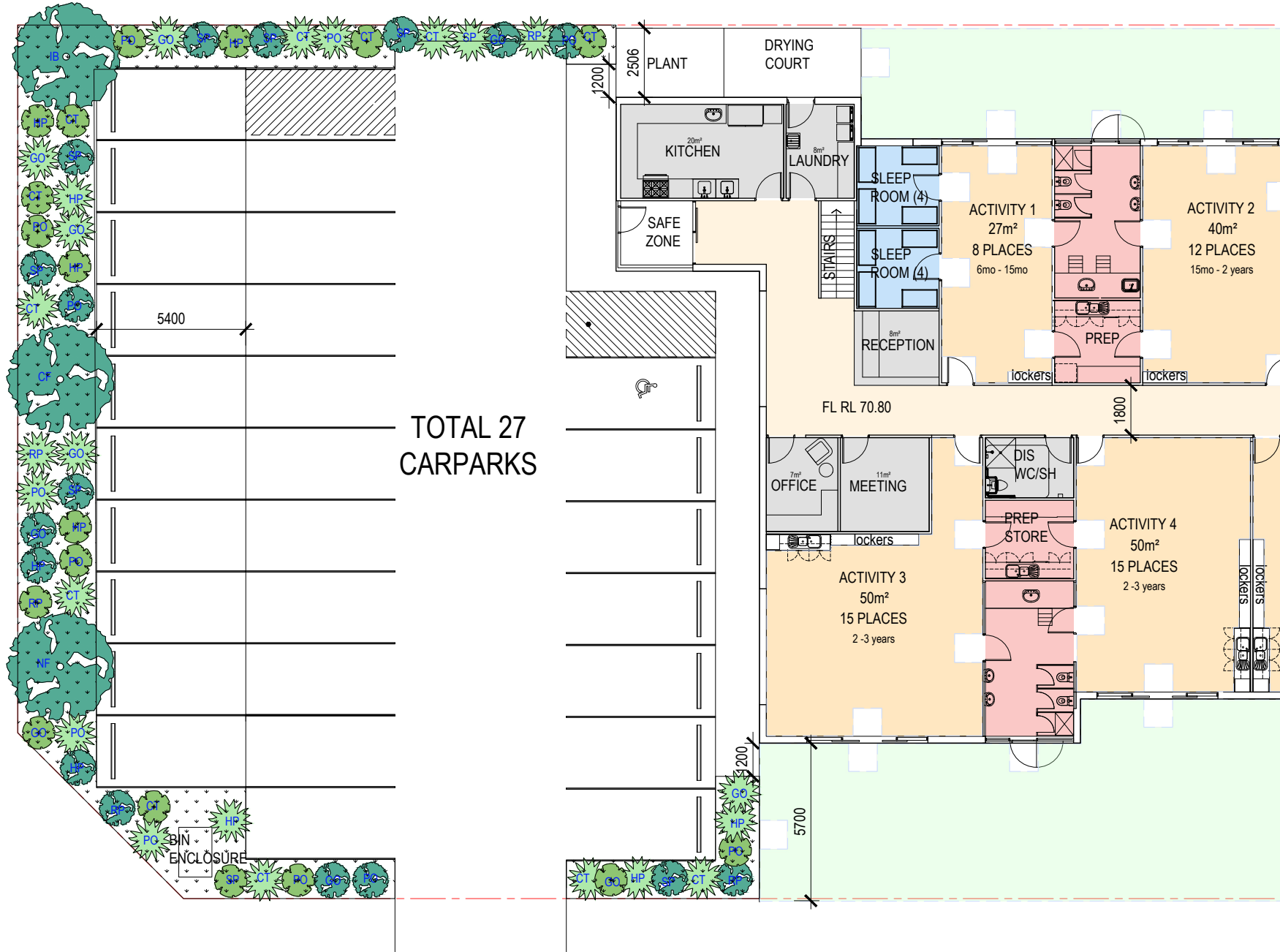
Project
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Kingsway, Landsdale WA

Drawing
PROPOSED ROOF PLAN

Scale As indicated Drawn XL
Client LEYTON PROPERTY
Date 04/08/2021
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Rev: - A3 SHEET

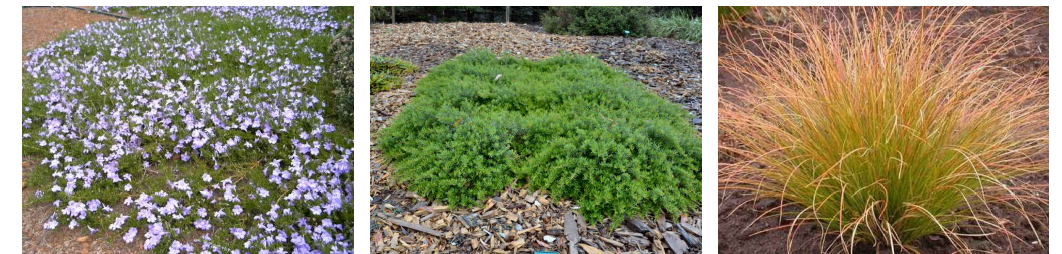


TREE IMAGES



NATIVE FRANGIPANI ILLAWARRA FLAME RED FLOWERING GUM RED IRON BARK TREE

PLANT IMAGES



SNAKE BUSH GIN GIN GUM CAREX



PURPLE FLAG LEUCOSPERMUM WEDDING BUSH

LANDSCAPING PLAN

1:200

PLANT SCHEDULE

KEY	BOTANICAL NAME	COMMON NAME	POT SIZE	SPACING	FULL SIZE
GRASSES					
HP	HEMIANDRA PUNGENS	SNAKE BUSH	140mm	3/m2	GROUND COVER
GO	GREVILLEA OBTUSIFOLIA	GIN GIN GEM	140mm	4/m2	GROUND COVER
CT	CAREX TESTACEA	CAREX	140mm	4/m2	400 X 450mm
PO	PATERSONIA OCCIDENTALIS	PURPLE FLAG	140mm	6/m2	400 X 600mm
SP	LEUCOSPERMUM SP	LEUCOSPERMUM	140mm	4/m2	500 X 400mm
RP	RICINOCARPUS PINIFOLIUS	WEDDING BUSH	140mm	4/m2	1M

TREE SCHEDULE

KEY	BOTANICAL NAME	COMMON NAME	POT SIZE	MAX SIZE (HEIGHT/CANOPY)
TREES				
IB	EUCALYPTUS SIDEROXYLON 'ROSE'	RED IRON BARK GUM	100L	10M/5M
CF	CORYMBIA FICIFOLIA	RED FLOWERING GUM	100L	12M/6M
NF	HYMENOSPORUM FLAVUM	NATIVE FRANGIPANI	100L	10M/5M
IF	BARCHYCHITON ACERFOLIUS	ILLAWARRA FLAME TREE	100L	20M/10M



Project
100 PLACE CHILD CARE FACILITY
 Kingsway, Landsdale WA

Drawing
LANDSCAPING PLAN

Scale 1:200 Drawn XL
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 Date 23/06/2021
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PRELIMINARY

Rev	Amendment	Date
-	-	-



NORTH ELEVATION
1:200



EAST ELEVATION
1:200



WEST ELEVATION
1:200



SOUTH ELEVATION
1:200



Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
PROPOSED ELEVATIONS

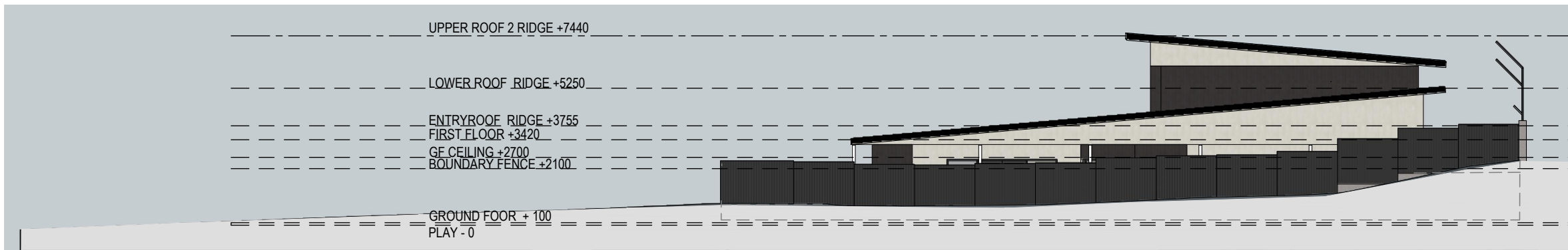
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Client LEYTON PROPERTY
Date 04/08/2021
Job No. 202100033
Dwg No. **DA07**



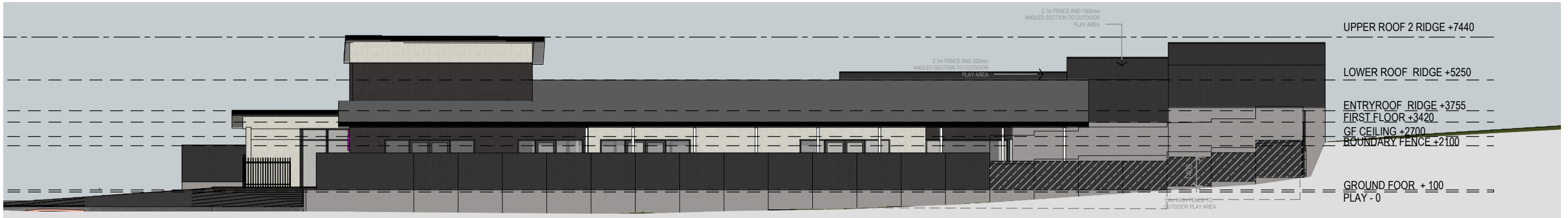
Rev: - A3 SHEET

PRELIMINARY

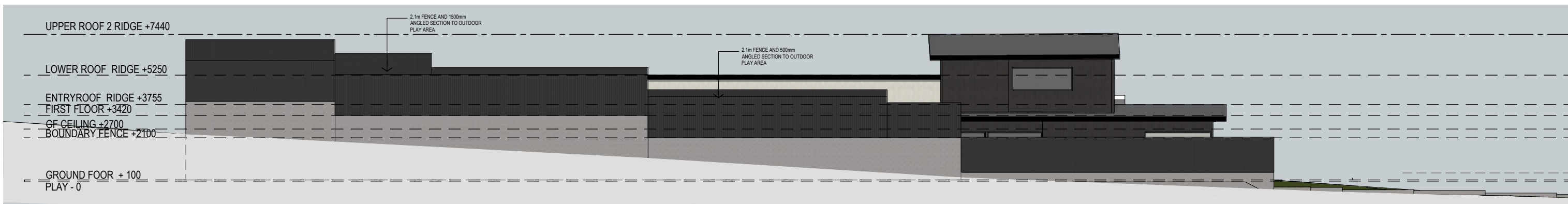
Rev	Amendment	Date
-	-	-



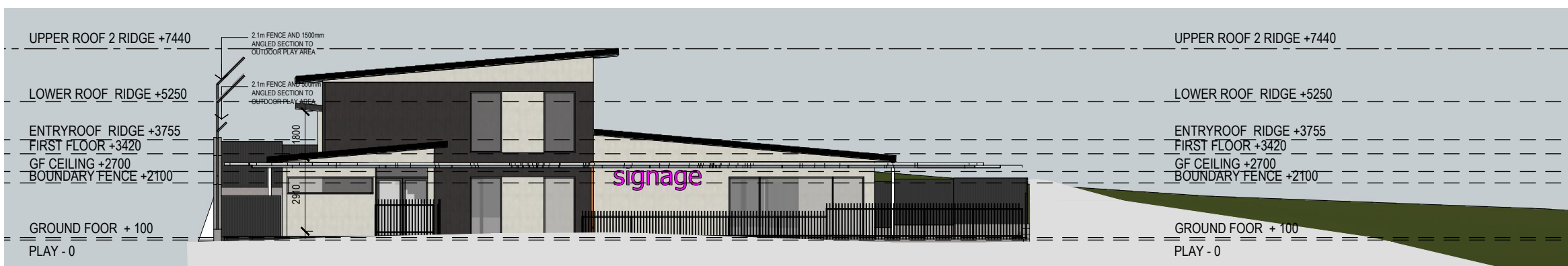
NORTH ELEVATION
1:200



EAST ELEVATION
1:200



WEST ELEVATION
1:200



SOUTH ELEVATION
1:200



Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
PROPOSED FENCE ELEVATIONS

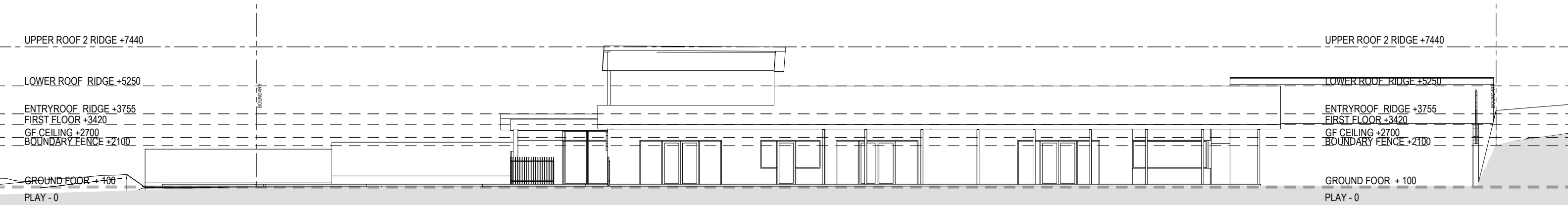
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Client	LEYTON PROPERTY		
Date	04/08/2021		
Job No.	202100033		
Dwg No.	DA08	Rev:	-



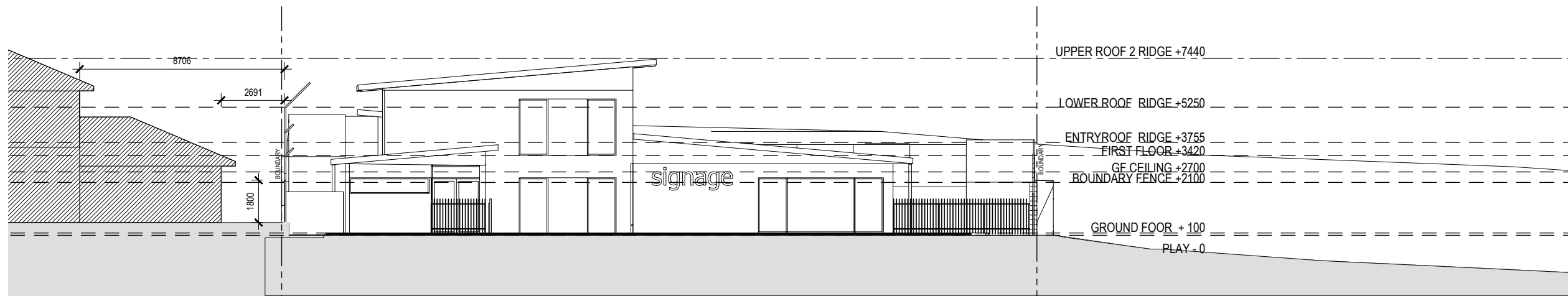
A3 SHEET

PRELIMINARY

Rev	Amendment	Date
-	-	-



SECTION A
1:200



SECTION B
1:200



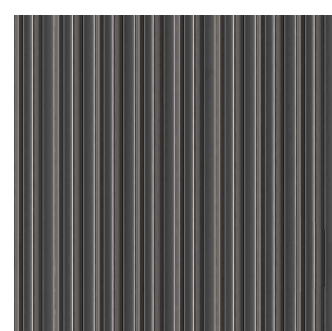
Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
SITE SECTIONS

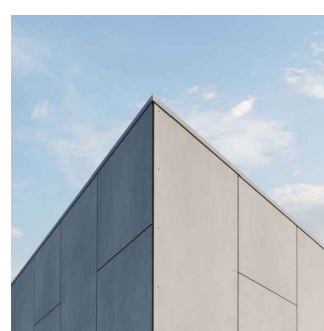
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Client	LEYTON PROPERTY		
Date	04/08/2021		
Job No.	202100033		
Dwg No.	DA09	Rev:	-



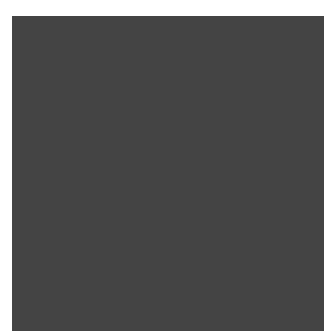
A3 SHEET



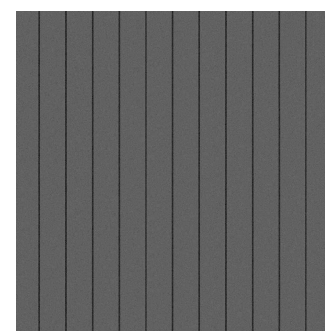
1 COLORBOND 'GOOD NEIGHBOURS' FENCE 2100 GUN METAL GREY



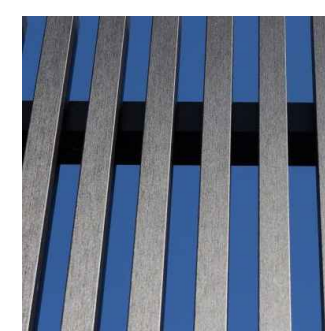
2 JAMES HARDIE EXOTEC PANEL WHITE PAINT FINISH



3 ALUMINIUM WINDOW FRAMES DULUX MONUMENT POWDERCOAT FINISH



4 JAMES HARDIE AXON CLADDING 'GRAIN' FINISH



5 BATTEN FENCE SYSTEM - BLACK



6 SOFFIT DULUX SNOWSEASON QUARTER



Project
100 PLACE CHILD CARE FACILITY
 Kingsway, Landsdale WA

Drawing
MATERIALS SELECTIONS

Scale	As indicated	Drawn	XL
Client	LEYTON PROPERTY		
Date	04/08/2021		
Job No.	202100033		
Dwg No.	DA10	Rev:	-

A3 SHEET

MATERIAL SELECTIONS



○ CONCEPT IMAGES

Project
100 PLACE CHILD CARE FACILITY
Kingsway, Landsdale WA

Drawing
CONCEPT IMAGES

Scale	As indicated	Drawn	XL
Client	LEYTON PROPERTY		
Date	04/08/2021		
Job No.	202100033		
Dwg No.	DA11	Rev:	- A3 SHEET



