

Proposed Change of Use Application

12 Hughie Edwards Drive, Merriwa

TRANSPORT IMPACT AND CAR PARKING ASSESSMENT

FINAL REPORT – V1

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1. INTRODUCTION

1.1 OVERVIEW

This Transport Impact and Car Parking Assessment has been prepared by Move Consultants on behalf of the Open Foundation with regard to a proposed Change of Use Application to locate a Place of Worship within the existing Chillisaurus Mini Golf tenancy within the existing commercial development located at 12 Hughie Edwards Drive, Merriwa in the City of Wanneroo. The site is broadly located in close proximity to the Ridgewood, Bulter and Quinns Rocks Activity Centre, east of Marmion Avenue and south of Lukin Drive within the south-west corner of Hughie Edwards Drive and Dalvik Avenue. The subject site houses a large building with four (4) existing tenancies inclusive of a restaurant, café, the subject tenancy (the mini golf tenancy) and a hair salon. This assessment has been prepared following receipt of a request sent to the Applicant in December 2023.

This assessment has been prepared with regard to the requirements set out by the City of Wanneroo and the WAPC *Transport Impact Assessment Guidelines – Volume 4: Individual Developments* as well as Council-endorsed strategies and guidelines, Australian Standards and Austroads guidelines.

1.2 SITE LOCATION

The site is broadly located in close proximity to the Ridgewood, Bulter and Quinns Rocks Activity Centre, east of Marmion Avenue and south of Lukin Drive within the south-west corner of Hughie Edwards Drive and Dalvik Avenue. The subject site houses a large building with four (4) existing tenancies inclusive of a restaurant, café, the subject tenancy (the mini golf tenancy) and a hair salon. Existing uses in place in the vicinity of the site are commercial uses immediately opposite the site on the east side of Dalvik Avenue, south of Hughie Edwards Drive and immediately west of the site abutting the east side of Marmion Avenue. Residential uses are in place broadly north of Hughie Edwards Drive to the immediate south- east and east of the site. The location of the site is shown in **Figure 1**.



Figure 1: Site Location

The general metropolitan context is shown in **Figure 2**.

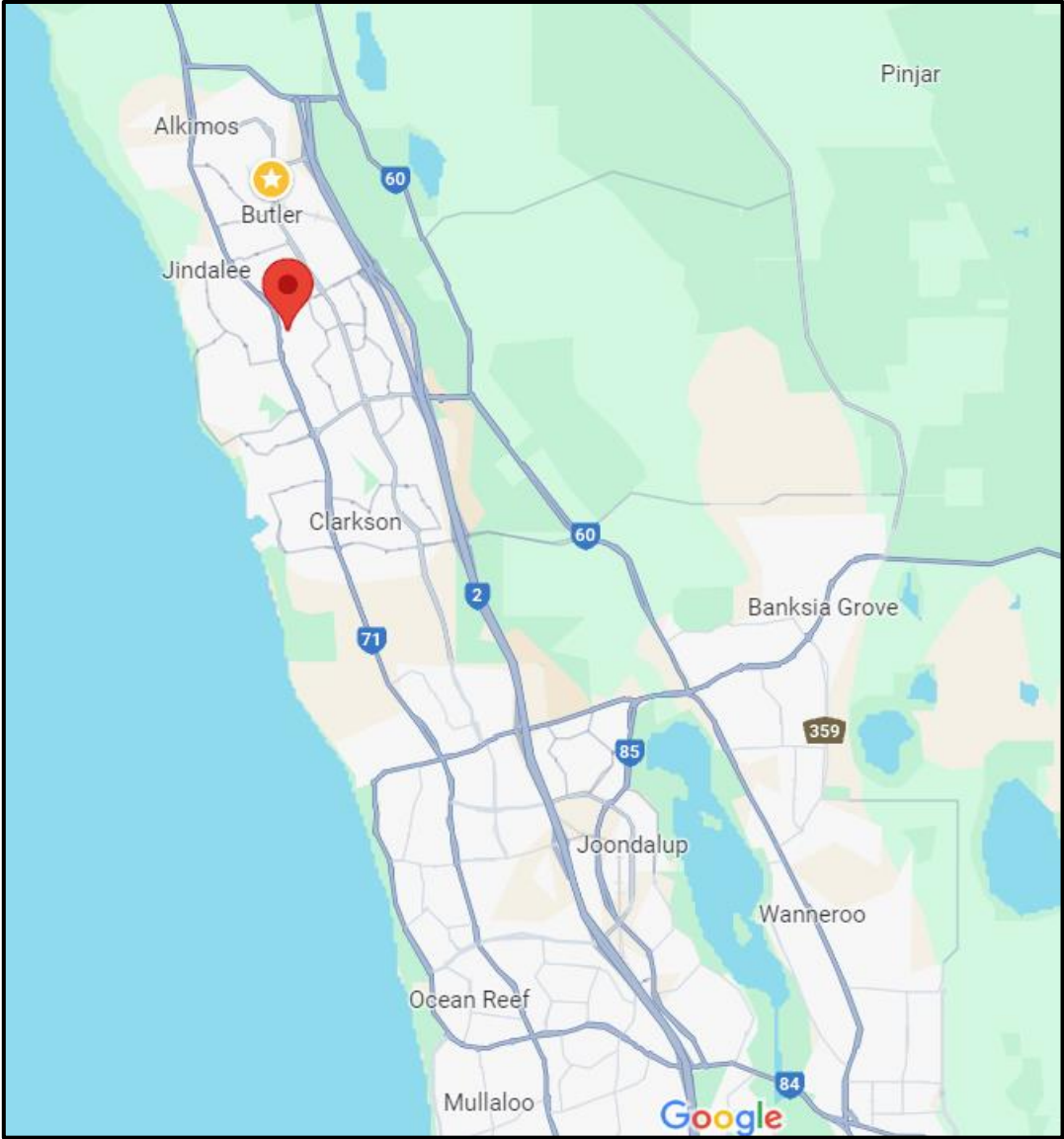


Figure 2: Metropolitan Context

1.3 SCOPE OF ASSESSMENT

This report has been prepared in accordance with the Western Australian Planning Commission's *Transport Assessment Guidelines: Volume 4 – Individual Developments* (2016).

Specifically, this report aims to assess the impacts of the proposed development on the boundary road network, namely within the local area to identify any modifications, to site or road layout, which may be required to serve the proposed site. In addition, an updated review of the anticipated on-site car parking requirements in relation to the Change of Use has also been undertaken and has been included in this updated assessment.

For this purpose, the traffic operations in the vicinity of the site crossovers have been considered and assessed under both existing and future proposed traffic conditions with regard to the potential impacts from changes in traffic generation associated with the Change of Use application.

2. EXISTING SITUATION

2.1 ROAD INFRASTRUCTURE

The site is located within the south-west corner of Hughie Edwards Drive and Dalvik Avenue, east of Marmion Avenue and south of Lukin Drive and approximately 140m east of the unsignalised T-intersection of Marmion Avenue and Hughie Edwards Drive. Existing uses in place in the vicinity of the site are commercial uses immediately opposite the site on the east side of Dalvik Avenue, south of Hughie Edwards Drive and immediately west of the site abutting the east side of Marmion Avenue. Residential uses are in place broadly north of Hughie Edwards Drive to the immediate south- east and east of the site.

Marmion Avenue, located to the west of the site, is a primary north-south connecting road providing direct access into the Quinns Rocks, Merriwa and Ridgewood activity centres and functions as a parallel high order reliever road to Connolly Drive and the Mitchell Freeway to the east of the site. It has been constructed as a dual divided carriageway to the west of the site with a raised central fixed median. It has been classified as a *Primary Distributor* road under the Main Roads WA *Functional Road Hierarchy* which are roads which “...provide for major regional and inter-regional traffic movement and carry large volumes of generally fast moving traffic with some as strategic freight routes and all are National or State roads.” The existing speed zoning on Marmion Avenue is 60 to 70kph to the west of the site and the road is owned, operated and maintained by Main Roads WA.

Lukin Drive, to the north of the site, is a major east-west connecting road between Marmion Avenue and Connolly Drive and functions as a primary route into and out of the Ridgewood and Quinns Rocks activity centres. It will eventually connect directly to the Mitchell Freeway via a full movements interchange which is currently under construction. It has been constructed as a dual divided carriageway north of the site. Connolly Drive, to the east of the site, functions as a parallel north-south reliever road to both Marmion Avenue to the west and the Mitchell Freeway to the east and provides direct access into the Butler Activity Centre. It has been constructed as a dual divided carriageway to the east of the site. Both roads have been classified as *District Distributor A* roads under the Main Roads WA *Functional Road Hierarchy* which are roads which “... carry traffic between industrial, commercial and residential areas and generally connect to Primary Distributors. These are likely to be truck routes and provide only limited access to adjoining property and are managed by Local Government.” The existing speed zoning on Lukin Drive is 60kph with the existing speed zoning on Connolly Drive set to 60kph north of Lukin Drive

and 70kph south of Lukin Drive. Both roads have also been designated as *Other Regional Roads* or *Blue Roads* under the *Metropolitan Region Scheme* and are owned, operated and maintained by the City of Wanneroo.

Hughie Edwards Drive, along the northern boundary of the site, and Dalvik Avenue, along the eastern boundary of the site, are secondary roads which provide transitional access from higher roads into the local road network in the area. Both roads have been roads which have been constructed as single undivided carriageways in the vicinity of the site and have been classified as *Local Distributor* roads under the *Main Roads Functional Road Hierarchy* which are roads which “...carry traffic within a cell and link *District Distributors* at the boundary to access roads with the route of the *Local Distributor* discouraging through traffic so that the cell formed by the grid of *District Distributors* only carries traffic belonging to or serving the area. These roads should accommodate buses but discourage trucks and are managed by *Local Government*.” Both roads operate under speed zoning of 50kph and are owned, operated and maintained by the City of Wanneroo.

The intersections of Marmion Avenue/Lukin Drive and Connolly Drive/Lukin Drive operate under signal control. The intersection of Marmion Avenue/Hughie Edwards Drive operates under unsignalised T-intersection full movements control with priority assigned to Marmion Avenue. The intersection of Hughie Edwards Drive/Dalvik Avenue is controlled by a 3-way single circulating roundabout.

Figure 3 shows the road hierarchy in the vicinity of the site.

A detailed site visit was conducted on Sunday 7th January 2024, Monday 8th January and Tuesday 9th January 2024 to collect information relating to existing road geometry, speed limits, and sightlines and to observe existing traffic operations on the adjacent boundary road network.

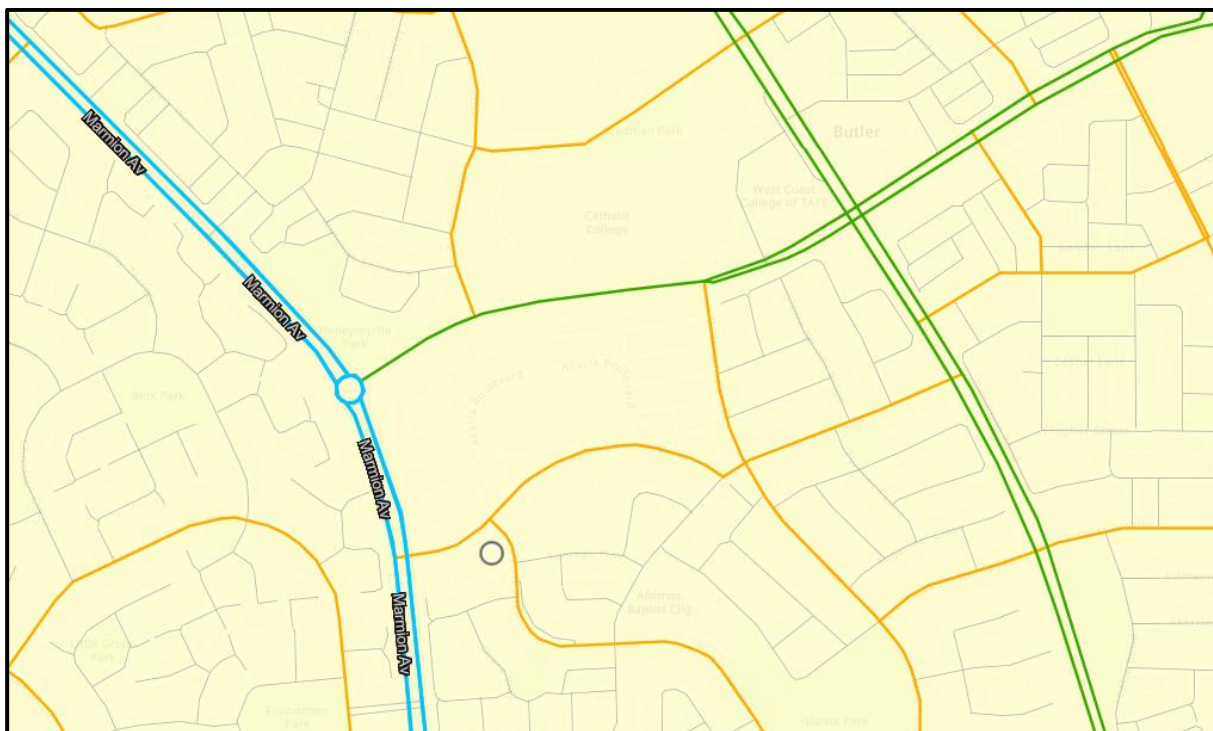


Figure 3: MRWA Functional Road Hierarchy – Local Road Network

Existing traffic volumes were obtained via data from Main Roads Western Australia for the road network in the vicinity of the site as well as derivation of volumes for both Hughie Edwards Drive and Dalvik Avenue based upon 'spot' surveys during peak period and a review of existing traffic patterns. **Table 1** shows the existing daily traffic volumes.

Table 1: Existing Traffic Volumes

Road Section	Daily Volume (vpd)	Source	Practical Capacity (vpd)
Marmion Avenue (North of Lukin Drive)	34,600 vpd	Main Roads WA (2022)	40,000 to 50,000 vpd
Lukin Drive (West of Connolly Drive)	10,300 vpd	Main Roads WA (2021)	25,000 vpd
Connolly Drive (North of Lukin Drive)	22,800 vpd	Main Roads WA (2022)	40,000 vpd
Connolly Drive (South of Lukin Drive)	27,100 vpd	Main Roads WA (2022)	40,000 vpd
Hughie Edwards Drive (East of Marmion Avenue)	Approx.. 6,000 vpd	N.A. (derived from 'spot' surveys and review of travel patterns)	7,000 to 10,000 vpd
Dalvik Avenue (South of Hughie Edwards Drive)	Approx.. 2,500 to 3,000 vpd	N.A. (derived from 'spot' surveys and review of travel patterns)	5,000 to 7,000 vpd

2.2 PUBLIC TRANSPORT, PEDESTRIAN, AND CYCLIST FACILITIES

The site is located approximately 200m due north-east of line haul bus services along Marmion Avenue to the west of the site with stops in place on both sides of the road south of Lukin Drive. Route 480 (Clarkson Station-Butler Station via Marmion Avenue) provides direct access to both Clarkson and Butler Railway Stations located to the south-east and north-east of the site, respectively with frequencies of 10 to 20-minutes during typical weekday peak periods and hourly service on Sundays and off-peak evening periods. Clarkson Station is located approximately

4.5m due south-east of the site and Butler Station is located 2.5km north of the site with service along the Joondalup Line providing 5 to 15-minute service frequencies during a typical weekday and 15 to 30-minute services on weekends and evenings.

Figure 4 show the existing public transport services in the area.

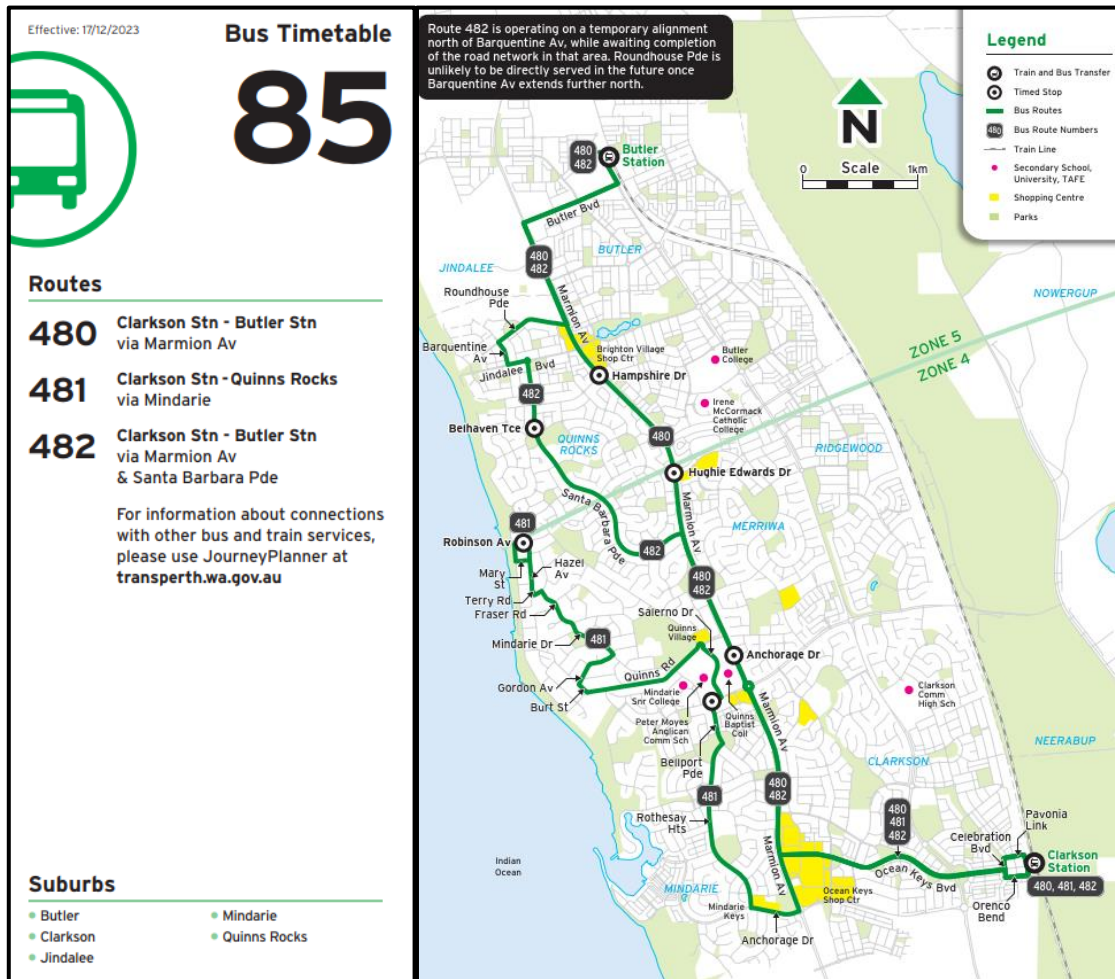


Figure 4: Existing Public Transport Bus Services

Bicycle lanes and/or sealed shoulders are in place on both sides of Marmion Avenue, Lukin Drive and Connolly Drive in the vicinity of the site with dual use off-road paths also in place on both sides of Marmion Avenue and Connolly Drive and on the north side of Lukin Drive. A dual use path is in place on the south side of Hughie Edwards Drive along the northern boundary of the site and on the east side of Dalvik Avenue to the east of the site.

Figure 5 shows the cycling and pedestrian infrastructure in the vicinity of the site.



Figure 5: Existing Cycling and Pedestrian Infrastructure

3. PROPOSED CHANGE OF USE

A plan of the existing site is attached in **Appendix A** showing the existing four (4) tenancy allocations across the site.....

3.1 PROPOSED ACTIVITY

The proposal seeks the approval of a Change of Use application for an existing tenancy at 12 Hughie Edwards Drive, Merriwa, namely the central tenancy between an existing hair salon and an existing restaurant. The existing café at the site within the central tenancy, currently occupied by a mini golf facility, will remain ancillary to the proposed place of worship. The proposed Change of Use entails the reallocation of the central mini golf tenancy to a place of worship.

The indicative hours of the proposal are as follows:

- Church services – Sunday morning – 10 a.m. to 12 p.m.(maximum 200 persons)
- Classes on Wednesday evening – 7 p.m. to 9 p.m. (maximum 50 persons)
- General office and administration – 9 a.m. to 5 p.m. (maximum 4 persons)
- Café – ancillary to worship, office and class activities (not open to public)

The operating hours above indicate that there will be limited times when the proposed place of worship will operate coincidentally with the other tenancies on the site (weekdays 9 a.m. to 4 p.m. with the exception of Mondays, Saturdays and Sundays for the hair salon and Wednesday evenings from 7 p.m. to 9 p.m. for the restaurant).

3.2 PROPOSED ACCESS AND PARKING ARRANGEMENTS

Existing access arrangements in place will continue to service the facility and include a full movements crossover to the south side of Hughie Edwards Drive and a full movements crossover on the west side of Dalvik Avenue. Reciprocal access is also in place between the site and existing commercial uses to the immediate west of the site. These crossovers measure 8.5 to 9.0m in width at the kerb line to the carriageway and narrow to 6m inside the property line. Existing car parking on the site consists of approximately 55 bays arranged in a right-angle manner abutting the northern, eastern and southern facades of the building. Additional car parking is in place adjacent to the site to the immediate west of the site in the order of approximately 39 bays.

3.3 END OF TRIP FACILITIES

End-of-trip facilities can be provided on the site on the site through the provision of bicycle U-rails if required external to the site or bicycle storage internal to the tenancy to be compliant with City of Wanneroo and Austroads guidelines.

4. TRANSPORT ANALYSIS

A traffic generation and distribution exercise has been undertaken to assess the potential traffic impacts associated with the proposed development. The aim of this exercise was to establish the traffic volumes which would be generated from the proposed development and to quantify the effect that the additional traffic has on the surrounding road network.

4.1 TRIP GENERATION AND DISTRIBUTION

The average amended daily traffic volume generated by the site was estimated based on traffic generation derived from the Institute of Transportation Engineers *Trip Generation Manual, 10th Edition* supplemented by information provided by the proponent with regard to the types of activity proposed to be undertaken on the site.

It should be noted that that café within the place of worship space will cater only to visitors to this use and not open to users of the gym.

Typical weekday traffic generation for the site has been estimated to be a maximum 50 daily vehicle trips (25 inbound/25 outbound), 3 vehicles trips during the a.m. peak hour (2 inbound/1 outbound) and 3 vehicle trips during the p.m. peak hour (1 inbound/2 outbound). Evening peak traffic generation for Wednesday between 7 p.m. and 9 p.m. would be in the order of 25 vehicle trips (20 inbound/5 outbound). The peak generating period for the place of worship will occur on a Sunday morning where typical peak hour generation would be in the order of 40 vehicle trips (35 inbound/5 outbound) with daily trips estimated on a Sunday to be in the order of 110 vehicle trips. Based upon a review of the existing operating protocols of the mini golf facility, this would result in a net change in traffic of approximately 5 vehicle trips per day and 2 additional trips during weekday peak hours plus 25 vehicles trips

between 7 p.m. and 9 p.m. on a Wednesday and 50 vehicle trips per day and 35 vehicle trips during the peak hour. This net difference has been derived following a review of existing operations at the site during peak demand periods and the existing operating hours of both the restaurant and the hair salon external to the proposal.

Based upon the existing traffic patterns in the area and the spatial distribution of adjacent land uses, the following distribution for the proposed development generated traffic has been assume broadly:

- 40% to and from the north via Marmion Avenue and Hughie Edwards Drive.
- 30% to and from the south via Marmion Avenue and Hughie Edwards Drive
- 30% to and from the east via Hughie Edwards Drive, Lukin Drive and Connolly Drive. Via Dalvik Avenue crossover.
- The number of trips entering / exiting the site via the proposed site crossover has been assigned based upon the most logical route for vehicles to take given their origin / destination.

The anticipated site-generated traffic was then assigned to the respective crossovers based upon the existing proportions for both the weekday a.m. and p.m. peak hours. The resultant increases in weekday daily and a.m. and p.m. peak hour-generated traffic under the 'worst case' scenario for the boundary road network would be as follows:

Weekday (Wednesday Peak Design Period):

- Marmion Avenue (North):
 - Daily: +2 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +10 vehicular trips
- Marmion Avenue (South):
 - Daily: +2 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +8 vehicular trips
- Lukin Drive:
 - Daily: +2 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +8 vehicular trips
- Connolly Drive:
 - Daily: +2 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +8 vehicular trips
- Hughie Edwards Drive:
 - Daily: +4 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +23 vehicular trips

- Dalvik Avenue:
 - Daily: +2 vehicular trips
 - A.M. Peak Hour: +1 vehicular trip
 - P.M. Peak Hour: +1 vehicular trip
 - Evening Peak Hour: +2 vehicular trips

Sunday:

- Marmion Avenue (North):
 - Daily: +20 vehicular trips
 - Peak Hour: +14 vehicular trips
- Marmion Avenue (South):
 - Daily: +15 vehicular trips
 - Peak Hour: +10 vehicular trips
- Lukin Drive:
 - Daily: +15 vehicular trips
 - Peak Hour: +10 vehicular trips
- Connolly Drive:
 - Daily: +15 vehicular trips
 - Peak Hour: +10 vehicular trips
- Hughie Edwards Drive:
 - Daily: +45 vehicular trips
 - Peak Hour: +32 vehicular trips
- Dalvik Avenue:
 - Daily: +5 vehicular trips
 - Peak Hour: +3 vehicular trips

These increases in the weekday daily and a.m./p.m. and evening peak hour and Sunday daily and peak hour volumes will have a minimal impact on existing traffic operations in the area and can be comfortably accommodated within the practical capacities of the respective links on the boundary road network.

These modest increases in daily and a.m./p.m. peak hour volumes will have a negligible impact on existing traffic operations in the area and can be accommodated well within the practical capacity of the boundary road network and will result in acceptable traffic operations on the adjacent road network.

In conclusion, it should be noted that based both on a review of the modelled revised total traffic assessment and observed traffic operations of the boundary road system, the anticipated site-generated traffic associated with the redevelopment proposal is negligible and that no external boundary road improvements will be required.

5. VEHICULAR ACCESS AND PARKING

5.1 ON-SITE QUEUING, CIRCULATION AND ACCESS AND CAR PARKING

The existing operating hours of the hair salon and restaurant are as follows:

- Hair Salon – Tues /Wed 9 a.m. to 4 p.m., Thursday 9 a.m. to 5:30 p.m. and Fri/Sat 9 a.m. to 5 p.m.
- Restaurant – Mon/Wed to Sunday – 4:30 p.m. to 9:30 p.m. (closed on Tuesdays)

The theoretical required car parking supply associated with the overall site is in the order of 18 bays as per the City of Wanneroo's *District Planning Scheme No. 2* is in the order of 53 bays

A review of the coincident maximum demand periods for all uses operating at full capacity on the site indicates the following:

- The place of worship and the hair salon parking demands will only overlap on weekdays between 9 a.m. and 5 p.m. with only office demand for car parking for the subject proposal required to be catered for.
- The place of worship and the restaurant parking demands would only overlap on weekdays between 4:30 p.m. and 5:00 p.m. and on Wednesday evenings from 7 p.m. to 9 p.m.
- Coincident demand for all tenancies on the site would only occur on weekdays between 4:30 p.m. and 5 p.m.
- Observations at the property on a Monday afternoon indicates a maximum peak demand on the site on a typical weekday at 4:30 p.m. of 10 vehicles parked on the site with a surplus of 45 bays.
- Observations at the property on a Tuesday afternoon indicates a maximum peak demand on the site on a typical weekday at 4:30 p.m. of 21 vehicles parked on the site with a surplus of 34 bays.
- Observations at the property on a Sunday morning indicates the maximum peak demand at 10 a.m. of 2 vehicles parked on the site with a surplus of 53 bays. It should be noted that this demand is associated with the mini golf facility which will be replaced by the place of worship. No demands were observed for parking associated with either the hair salon or the restaurant as these tenancies do not operate during this time period.
- It is anticipated that the parking demand on a Wednesday evening would be in the order of 15 to 20 vehicles associated only with the restaurant on the site as the hair salon and existing mini golf tenancies are closed.

The City of Wanneroo's TPS indicates that a requirement of in the order of 53 car parking bays are theoretically required to cater to the place of worship. Based upon a review of the TPS and the existing demands associated with the tenancies on the site, it is anticipated that the car parking demands for a typical weekday between 9 a.m. and 5 p.m. would be in the order of 4 bays, for a typical Wednesday evening would be 15 to 20 bays and 27 bays on a Sunday morning. These demands can be comfortably accommodated within the existing surplus supply on the site based upon the 'worst case' scenario (not taking into consideration the car parking demands associated with the existing mini golf operation). This efficient and effective use of car parking supply on the site is a result of complimentary uses on the site which do not generate their respective cumulative peak parking demands during the same time periods. In addition, the car park adjacent to the site immediately to the west which shares reciprocal access to the boundary road network was observed on a Sunday morning to have only 1 vehicle parked between 10 a.m. and 11 a.m. and there is potential for the Applicant to enter into discussions with the adjacent land owner to access this car parking if required.

A review of the proposed car parking layout was undertaken to assess the adequacy of the proposed site access and circulation in addition to service/delivery areas on the site. The design of the proposed car parking on the site has been reviewed using traffic engineering standards and the relevant Australian Standards and Austroads

guidelines, with the proposed design considered adequate to accommodate on-site manoeuvring and circulation with all vehicles entering and exiting the car parking areas in forward gear from and to the boundary road network.

It can therefore be concluded that the existing car parking supply on the site will adequately cater to coincident peak parking demands across the tenancies on the site with no additional car parking required. This is consistent with the City's TPS and traffic engineering best practice. This approach proposed by Move Consultants has also been endorsed by sitting members with regard to number of applications approved by the State Administrative Tribunal and is considered a reasonable and appropriate cost-effective approach to the provision and management of public car parking infrastructure.

Service, delivery and loading activities will continue to be accommodated within the existing delivery/loading area at the rear of the site and details can be negotiated with the City of Wanneroo.

5.2 SIGHTLINE REVIEW AND CRASH HISTORY

A review of the sightlines along Hughie Edwards Drive and Dalvik Avenue along the northern and eastern boundaries of the site, respectively, in the vicinity of the site crossovers for exiting and entering vehicles has been undertaken and it can be concluded that due to the low-speed environment, the sightlines meet minimum Austroads sightline requirements for MGSD, ASD and SISD.

An updated review of the crash history for the 5-year reporting period 2018-2022 of indicates all crashes recorded for this time period occurred at the intersection of Marmion Avenue/Hughie Edwards Drive with no driveway or midblock crashes along the respective frontages of the site. This indicates that the additional traffic associated with the development will have a negligible impact on the risk profile on the boundary road network and can accommodate pedestrians, cyclists, and public transport users safely to and from the site as well as vehicles parking on and off the site.

6. CONCLUSIONS

This Transport Impact and Car Parking Assessment has been prepared by Move Consultants on behalf of the Open Foundation with regard to a proposed Change of Use Application to locate a Place of Worship within the existing Chillisaurus Mini Golf tenancy within the existing commercial development located at 12 Hughie Edwards Drive, Merriwa in the City of Wanneroo. The site is broadly located in close proximity to the Ridgewood, Bulter and Quinns Rocks Activity Centre, east of Marmion Avenue and south of Lukin Drive within the south-west corner of Hughie Edwards Drive and Dalvik Avenue. The subject site houses a large building with four (4) existing tenancies inclusive of a restaurant, café, the subject tenancy (the mini golf tenancy) and a hair salon. This assessment has been prepared following receipt of a request sent to the Applicant in December 2023.

A review of the anticipated amended traffic generation associated with the proposal indicates that the expected traffic which will be generated by the development on a daily basis and during peak weekday a.m., p.m., and evening as well as Sunday daily and peak demand periods can be comfortably accommodated within the practical capacity of the boundary road network with no impacts expected to existing traffic operations.

A review of the proposed car parking layout was undertaken to assess the adequacy of the proposed site access and circulation in addition to service/delivery areas on the site. The design of the proposed car parking on the site has been reviewed using traffic engineering standards and the relevant Australian Standards and Austroads guidelines, with the proposed design considered adequate to accommodate on-site manoeuvring and circulation with all vehicles entering and exiting the car parking areas in forward gear from and to the boundary road network.

Service, delivery and loading activities will continue to be accommodated within the existing delivery/loading area at the rear of the site and details can be negotiated with the City of Wanneroo.

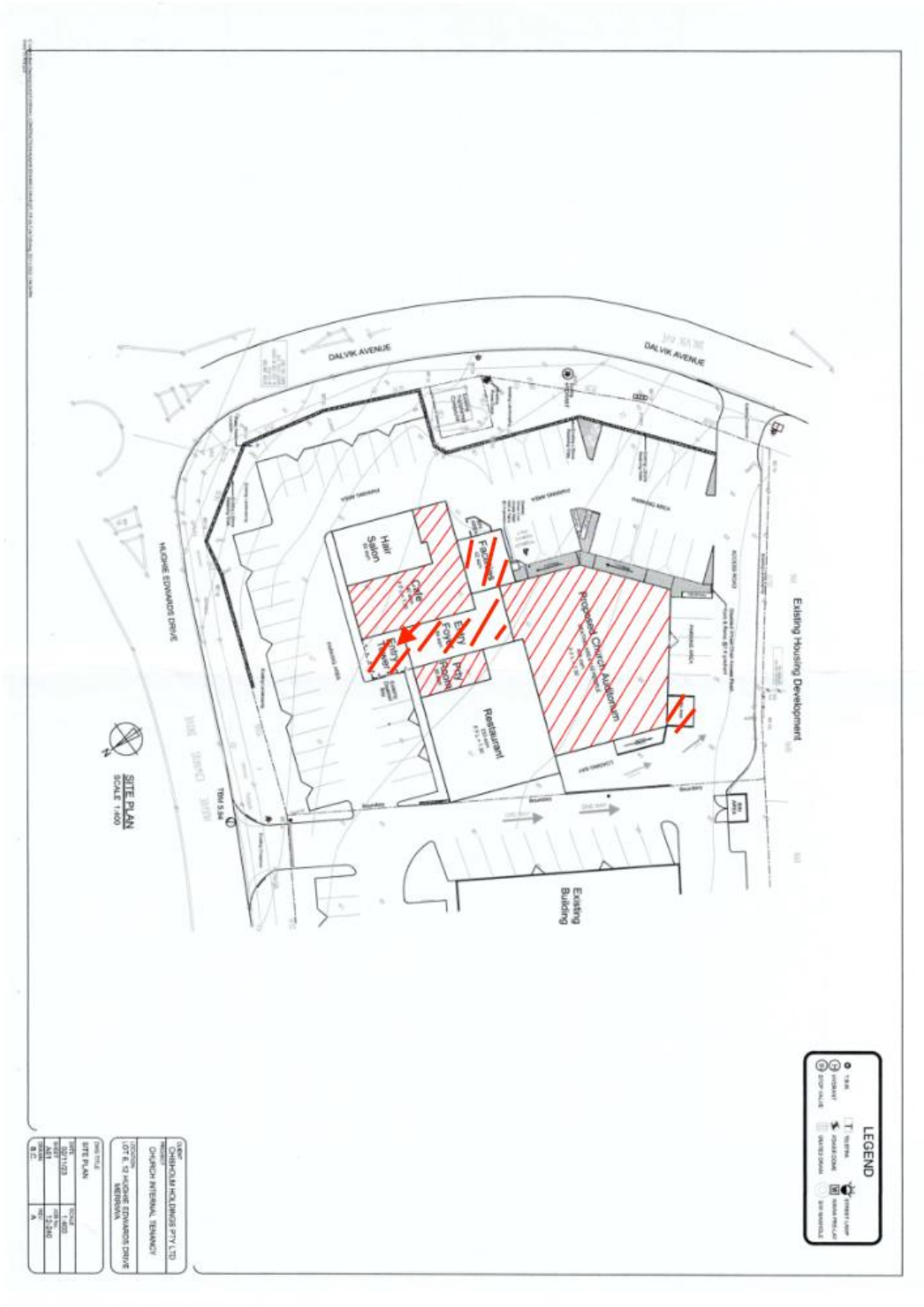
The City of Wanneroo's TPS indicates that a requirement of in the order of 53 car parking bays are theoretically required to cater to the place of worship. Based upon a review of the TPS and the existing demands associated with the tenancies on the site, it is anticipated that the car parking demands for a typical weekday between 9 a.m. and 5 p.m. would be in the order of 4 bays, for a typical Wednesday evening would be 15 to 20 bays and 27 bays on a Sunday morning. These demands can be comfortably accommodated within the existing surplus supply on the site based upon the 'worst case' scenario (not taking into consideration the car parking demands associated with the existing mini golf operation). This efficient and effective use of car parking supply on the site is a result of complimentary uses on the site which do not generate their respective cumulative peak parking demands during the same time periods. In addition, the car park adjacent to the site immediately to the west which shares reciprocal access to the boundary road network was observed on a Sunday morning to have only 1 vehicle parked between 10 a.m. and 11 a.m. and there is potential for the Applicant to enter into discussions with the adjacent land owner to access this car parking if required.

It can therefore be concluded that the existing car parking supply on the site will adequately cater to coincident peak parking demands across the tenancies on the site with no additional car parking required. This is consistent with the City's TPS and traffic engineering best practice. This approach proposed by Move Consultants has also been endorsed by sitting members with regard to number of applications approved by the State Administrative Tribunal and is considered a reasonable and appropriate cost-effective approach to the provision and management of public car parking infrastructure.

In conclusion, it should be noted that based both on a review of the modelled total traffic and observed traffic operations of the boundary road system, the anticipated site-generated traffic associated with the proposed development can be accommodated within the existing practical capacity and functional road classification of the local road system and the existing car parking supply on the site can adequately cater to the proposal's car parking demands with the proposed Change of Use application wholly justified based upon endorsed traffic engineering principles.

APPENDIX A: SITE PLAN

PLAN OF LEASED PREMISES



Landlord initial here _____

Tenant initial here _____