



**Amendment No. 03 to the
North Eglinton Agreed
Structure Plan No. 93**
December 2023

Record of amendments made to the North Eglinton Agreed Structure Plan No. 93

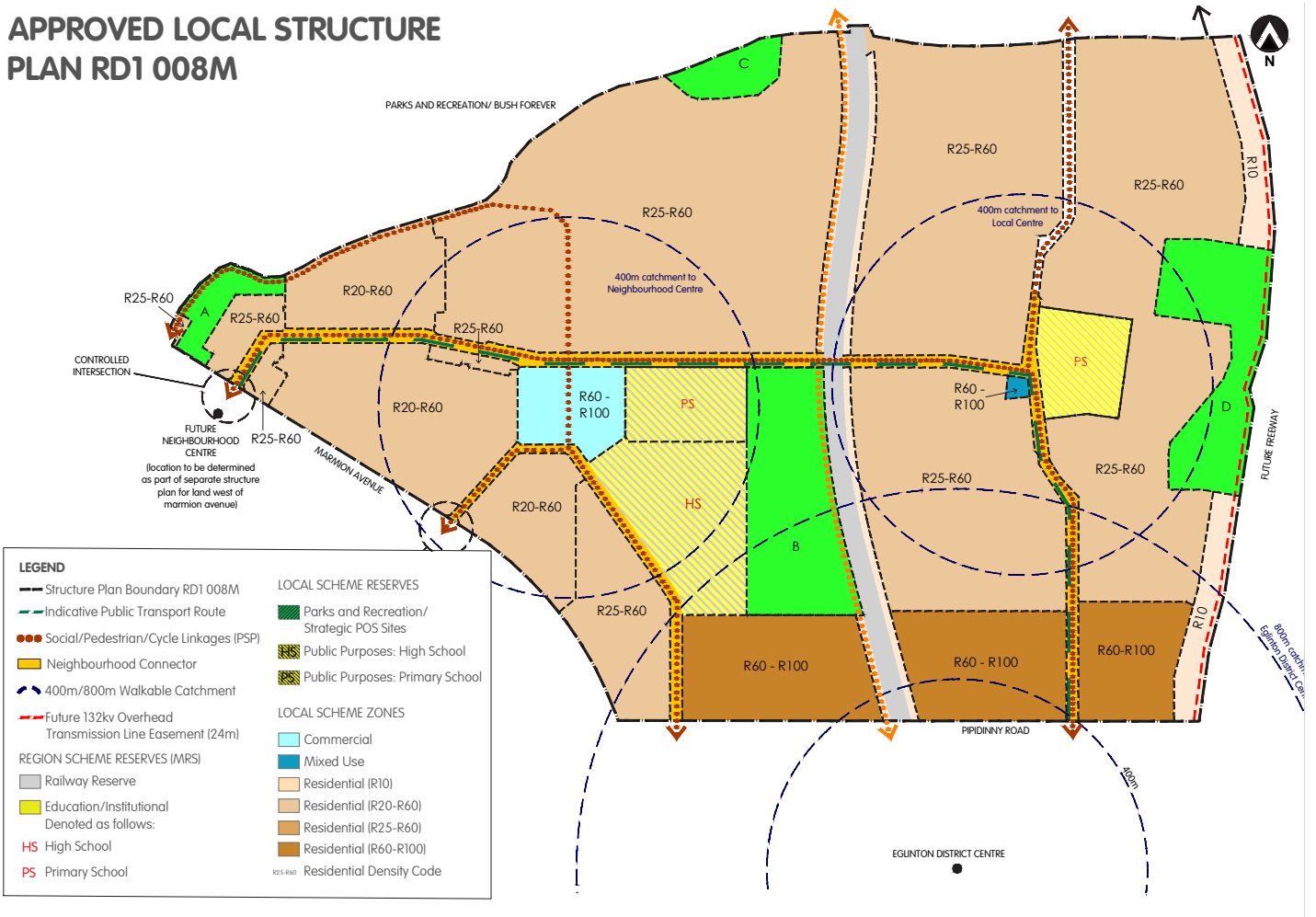
Amendment No.	Summary of the Amendment	Date approved by WAPC
01	<p>Incorporate provisions under Part One to allow for the implementation of the R-MD Codes within the Structure Plan area via the City of Wanneroo adopted Local Planning Policy 4.19: Medium Density Housing Standards (R-MD).</p> <p>Increase the base density code within the Structure Plan area to R25 to facilitate application of the R-MD provisions.</p> <p>Update the format of Part One to accord with the requirements of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> (Regulations) and the Western Australian Planning Commission's (WAPC) Structure Plan framework.</p> <p>Incorporate agreed modifications to the distribution of 'conservation' open space within the Structure Plan area.</p>	30 January 2019
02	<p>Include text into Part 1, Section 4.1 to provide for a combination of temporary uses at Lot 9502 (HN 5) Shiny Lane, Eglinton</p>	11 May 2020
03	<p>Modify the south-western precinct of the Structure Plan Map as follows:</p> <ol style="list-style-type: none"> 1. Relocate the Indoor Recreation Centre to Pippidiny Road and rezone the current site to Mixed Use; 2. Relocate the District Open Space and the high school adjacent the Railway Line and introduce a second north-south neighbourhood connector; 3. Modify the format of the primary school; 4. Introduce a Service Commercial zone at the north-eastern intersection of Marmion Avenue and Pippidiny Road; and 5. Remove the high-density residential precinct adjacent Pippidiny Road and amend the design of the residential area to reflect the above changes. 	

Amendment No. 03 to the North Eglinton Approved Structure Plan No. 93

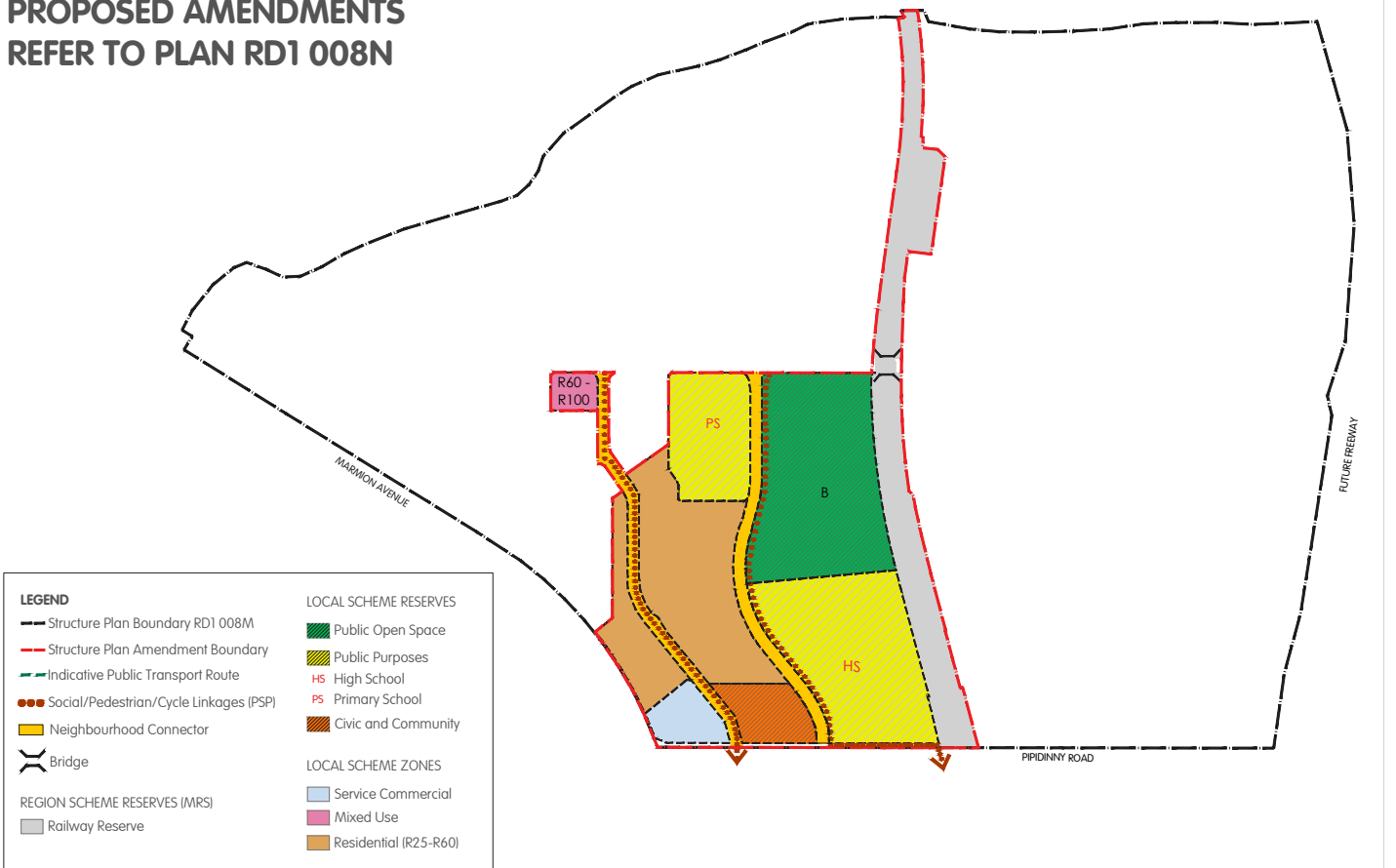
The City of Wanneroo, pursuant to its District Planning Scheme No. 2, hereby recommends to the Western Australian Planning Commission to approve the abovementioned amendment by:

1. Inclusion of an Executive Summary
2. Amending Part One – Implementation as follows:
 - a) Section 5.1 – Local Development Plan
Amending and including the following:
 - g. the entire ‘Commercial’ zone in the form of one Local Development Plan for the entire area;
 - h. each ‘Mixed Use’ zone in the form of one Local Development Plan for each area;
 - k. the entire ‘Service Commercial’ zone in the form of one Local Development Plan for the entire area.
3. Amending Part Two – Explanatory as follows:
 - a) Relevant updates throughout to reflect the format and terminology set out in the WAPC’s “WA Planning Manual – Guidance for Structure Plans” where this is necessary for the effective operation of the Structure Plan.
 - b) Section 1 – Update cadastral details.
 - c) Section 2 – Minor updates to relevant planning instruments to reflect current legislative context.
 - d) Sections 3.3.3 and 4.4.6 – Activity Centres and Employment: Incorporate summary of new Economic Assessment Report.
 - e) Section 4 – Local Structure Plan: Update where required to reflect new structure plan design and planning proposals.
 - f) Section 4.4.3 – Public Open Space and Landscape Strategy: Update POS Schedule and plan to reflect new structure plan design.
 - g) Section 4.5 – Movement Network: Update with summary of new Transport Impact Assessment.
 - h) Table 11 – Table 11, Pre-lodgement consultation to be updated with engagement summary relating to Amendment No. 3.
 - i) Section 8 – Summary of Technical Reports: Supplement with findings of updated technical reporting (where relevant)
 - j) Update relevant figures throughout to reflect the new Structure Plan design.
4. Replace the current Structure Plan Map (Plan 1-SAT EGL RD1 008M) with new Revision SAT EGL RD1 008N which includes the following major modifications:
 - a) Relocate the Indoor Recreation Centre to Pipidinny Road and rezone the current site to Mixed Use;
 - b) Relocate the District Open Space and the High School adjacent the Railway Line and introduce a second north-south neighbourhood connector;
 - c) Amend the extent of the Railway Reserve;
 - d) Modify the format of the primary school;
 - e) Introduce a Service Commercial zone at the north-eastern intersection of Marmion Avenue and Pipidinny Road; and
 - f) Remove the high-density residential precinct adjacent Pipidinny Road and amend the design of the residential area to reflect the above changes.

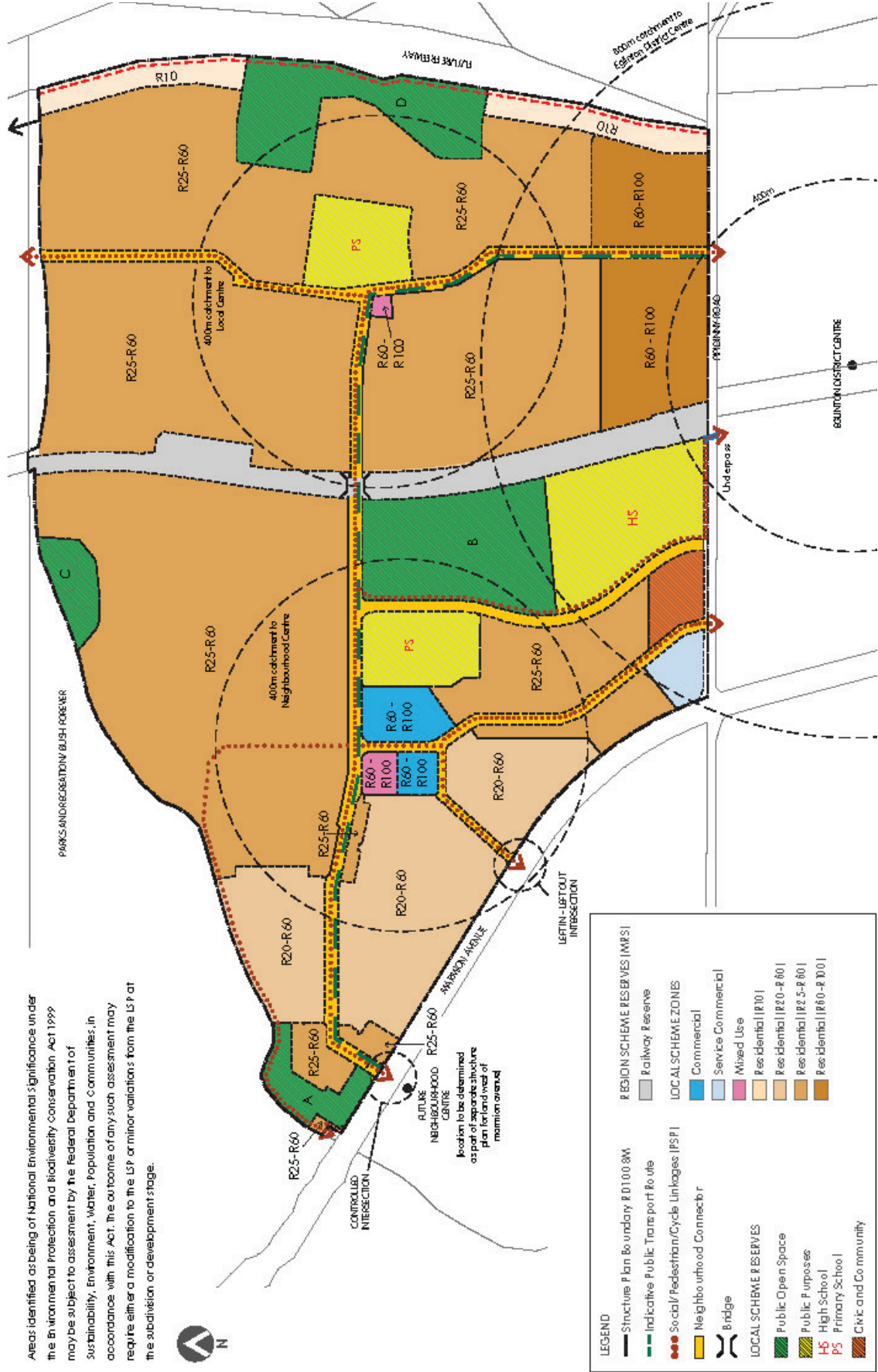
APPROVED LOCAL STRUCTURE PLAN RD1 008M



PROPOSED AMENDMENTS REFER TO PLAN RD1 008N



Plan 1 inclusive of Amendment No.3 Modifications (RD1008N)



Endorsement Page

This Structure Plan Amendment is prepared under the provisions of the
City of Wanneroo District Planning Scheme No. 2

IT IS CERTIFIED THAT THIS STRUCTURE PLAN AMENDMENT NO. 03 TO THE
NORTH EGLINTON APPROVED STRUCTURE PLAN NO. 93
WAS APPROVED BY
RESOLUTION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION ON

.....

Date

Signed for and on behalf of the Western Australian Planning Commission

.....

an officer of the Commission duly authorised by the Commission pursuant to section 24 of the *Planning and
Development Act 2005* for that purpose, in the presence of:

.....

Witness

.....

Date

.....

Date of Expiry



Structure Plan Amendment Report

Amendment No. 03 to the
North Eglinton Agreed Structure Plan No. 93



Executive Summary

Hatch RobertsDay act on behalf of Development WA and Satterley Property Group in preparing this amendment to the North Eglinton Agreed Structure Plan No. 93. The amendment introduces an improved road network and urban structure, responding to external changes in the movement network on and adjacent to Pipidinny Road associated with the new PTA carpark and the Eglinton District Centre.

The key purposes of the amendment are to:

1. Relocate the future 'Indoor Recreation Centre' (IRC) to improve its proximity to the Eglinton Railway Station, as requested by the City of Wanneroo.
2. Reconfigure the layout of the Primary School, High School and District Playing Fields to better reflect agency operational requirements and staging and timing considerations for development of the Allara Estate.
3. Introduce a new Service Commercial zone at the intersection of Pipidinny Road and Marmion Avenue which is a better land-use fit for the modified conditions resulting from changes to the Eglinton District Centre area and the delivery of works associated with delivery of the new Eglinton Railway Station.
4. Update relevant elements of the Structure Plan report to reflect the format and terminology prescribed in the WAPC's State Planning Policy 7.2 Precinct Plan Manner and Form where this is necessary for the effective operation of the Structure Plan.

Approval to the modifications will establish a firm basis for the continued development of Allara. In the short term this includes development of the Primary School (2026), the City's delivery of the Indoor Recreation Centre (scheduled for 2027) and a final stage of residential development west of the railway line (2024). Medium term, delivery of the District Playing Fields via the Alkimos-Eglinton Development Contribution Scheme will create another regional level attraction within the Estate, rounded out in the longer term by the High School.

The Amendment is supported by an updated suite of technical reporting which demonstrates that the proposed modifications can be accommodated within the current management approach of the adopted bushfire, traffic and hydrological strategies for the Estate.

1.0 Background

The North Eglinton Agreed Structure Plan No. 93 (ASP93) was approved by the Western Australian Planning Commission (WAPC) in June 2014. ASP93 was subsequently amended in January 2019 and again in May 2020.

Discussions around the requirement for a further amendment to the Structure Plan commenced in late 2020, with the City requesting the JV's consideration to review the location of the Indoor Recreation Centre, currently proposed in the Allara Neighbourhood Centre. The City asked the JV to investigate whether there was a suitable location for this facility that could enable both an increase in the size of the site from the current 0.70ha to a minimum 1.5 ha, and a location closer to the walkable catchment of the Eglinton Train Station and District Centre. The JV accordingly commenced a series of design investigations to consider potential locations and the related design amendments to the balance of the Structure Plan to accommodate the change.

Subsequent to this, PTA via the Newest Alliance, responsible for delivery for the Northern Rail line, engaged with the JV to request consideration of the acquisition of additional land adjacent the railway line to facilitate necessary design modifications to the project.

An iterative design process commenced with a series of workshops and liaison with key agencies, including the City of Wanneroo, the Department of Planning, Lands and Heritage, the PTA / Newest Alliance and the Department of Education, to reach agreement on the proposed modifications documented in this amendment request.

2.0 Proposed Amendment

The key purpose of the structure plan amendment is the relocation of key institutional sites within the structure plan area and resulting modifications to the balance of the affected area. The scope of and rationale for the proposed modifications is detailed below.

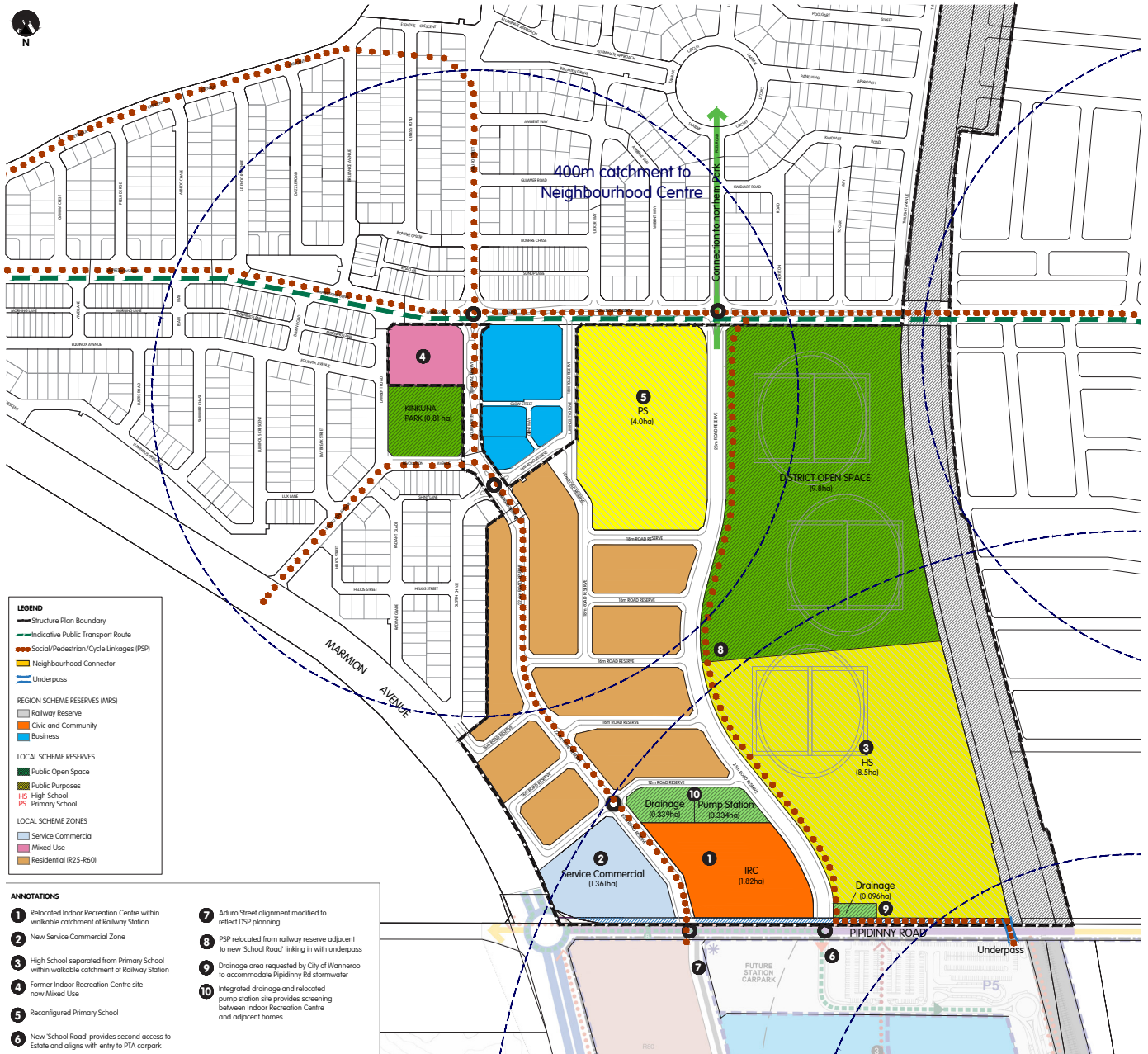
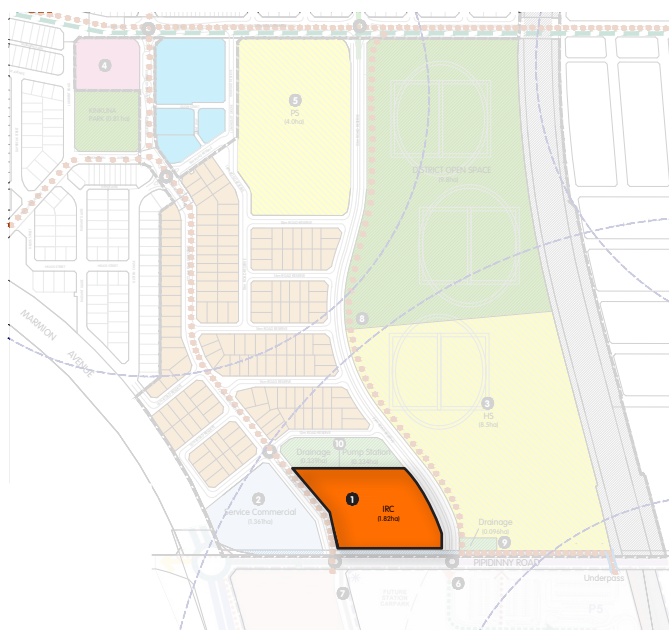


Figure 1 - Structure Plan Amendment No.3 Modifications



2.1. Indoor Recreation Centre

The current site of the Indoor Recreation Centre is located on Aduro Street and adjacent Kinkuna Park. However, further to the City’s request for a larger site closer to the railway station, the new 1.82ha site on Pipidiny Road has been selected and agreed with the City.

The new site is located near to the 400 metre walkable catchment of the new railway station. It has good direct access from both the internal road network south of Pipidiny Road, as well as via the underpass adjacent the railway line which would require a single road crossing to access the facility.

The site achieves the City’s requirements for a site greater than 1.50 hectares, comprising a 1.82 hectare site which is able to comfortably accommodate a significantly larger Indoor Recreation Centre from that originally envisaged for Allara. The site provides a high visibility location with suitable area to accommodate both parking and generous landscaping appropriate to this high-profile entry to the Estate. The JV’s expectation is for the delivery of a high-quality public facility that celebrates this important location.

The new IRC site is proposed to be reserved ‘Civic and Community’ aligning with its intended purpose. The existing site within the Village Centre is proposed to be rezoned to ‘Mixed Use’, providing flexibility for either residential development which is in high demand, commercial uses or an alternative form of community purpose.



2.2. High School and District Open Space

a. Location and Configuration

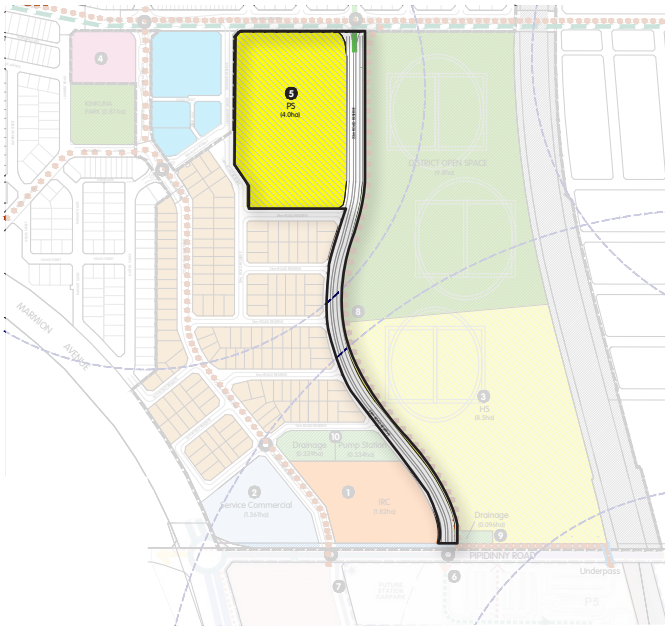
The Amendment proposes to separate the Primary School and High School sites in line with the operational preference of the Department of Education, with a consequent modification to the configuration of the District Open Space. The High School is relocated adjacent Pipidiny Road and the railway line, bringing the facility into the 400 metre walkable catchment of the Eglinton Railway Station.

To accommodate the change the District Open Space is also reconfigured in its same general location. Both facilities maintain the same effective area of 9.80ha and 8.50ha depicted in the current Structure Plan to implement the current shared use arrangement of sporting facilities.

b. Department of Education Feedback

The JV liaised extensively with the Department of Education to arrive at the preferred design solution, balancing the Department’s operational preferences with a variety of other constraints that have influenced the final design. The Department has provided a range of detailed feedback, some of which has been accommodated in the Structure Plan design and other matters which will be considered incorporated as subdivision and development progresses.

In the main the Department “notes a substantial improvement in the design and commends the applicant for reconfiguring the precinct layout to be more conducive to OP 2.4 school planning requirements.” Principally, physical separation of the high school and primary school sites and the ability to utilise the easily accessible and legible Pipidiny Road as the main entry point. As noted below, the introduction of the new ‘School Road’ provides an additional access option, with general residential traffic now able to be separated from school traffic during peak periods.



2.3. Primary School

The proposed Primary School adjacent the Village Centre remains in its same general location with a modified configuration. The school maintains its role as an integral part of the Village, preserving its strong pedestrian connection via Glow Street / Blaze Lane to the main street and Kinkuna Park. The design depicted in the Structure Plan aligns with that depicted in a recently lodged subdivision for the facility.

The configuration and context of the primary school site is consistent with the requirements of WAPC Operational Policy 2.4, comprising a 4ha site with roads on all sides. Road reserves adjacent the school are a minimum 18 metres, enabling the provision of embayed parking and pedestrian/cycling paths be constructed. This is a substantial improvement to the current design response which provides for only two road interfaces to the west and north of the site, with the Primary School closely integrated with the future High School.

The revised design reflects the stated preference of the Department of Education during the pre-lodgement consultation to move away from the co-location of public primary and secondary educational facilities, as currently depicted in the endorsed Structure Plan.



2.4. New School Road

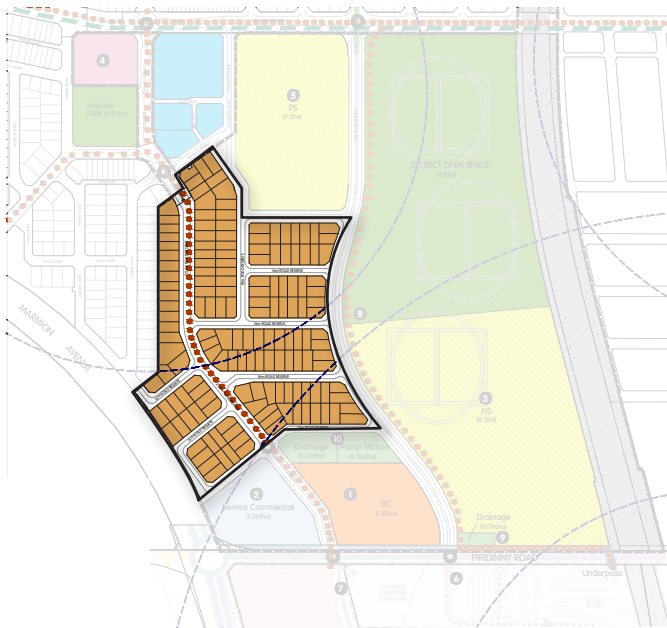
Relocation of the schools and the District Open Space allows for the introduction of a second north-south neighbourhood connector within the precinct, connecting from Pipidinny Road through to Impressions Drive, and through to the major park (POS 12) in the northern portion of the Estate.

The alignment of the new neighbourhood connector takes the form of a ‘parkland boulevard’ with distinctive curves and deflections. In practical terms this design response is essential to accommodate the area requirements of the adjacent ovals and High School and to align with the fixed access points to Impressions Drive and the PTA carpark access off Pipidinny Road.

Introduction of the new ‘School Road’ has a number of advantages to the current single access road through this precinct in the endorsed Structure Plan. This includes:

1. Better alignment with Liveable Neighbourhood recommendations, in particular creating an institutional road that services the 2 schools and the District Open Space;
2. Enables primary traffic, including public transport, for these facilities to be separated from local traffic accessing the Allara Village Neighbourhood Centre;
3. Provides more access and parking options for both the Primary School and High School;
4. Creates a strong, direct connection from the Eglinton Railway Station, to the schools, IRC and District Open Space, as well as the northern open space area and pedestrian / cycle / POS network. The approved previous design loads all northern connections onto the neighbourhood connector running through the Village Centre;
5. Formalises the relationship between the Primary School and the District Open Space with a public road, whereas the current concept relied on the development of an internal road network for the various institutional uses; and
6. From a place and character perspective, introduces an under-represented road typology in the north-west corridor and more diversity in streetscapes within the Estate. This provides some visual differentiation from the more utilitarian grid that has been applied elsewhere within the Estate and a more informal feel and character to this important street.





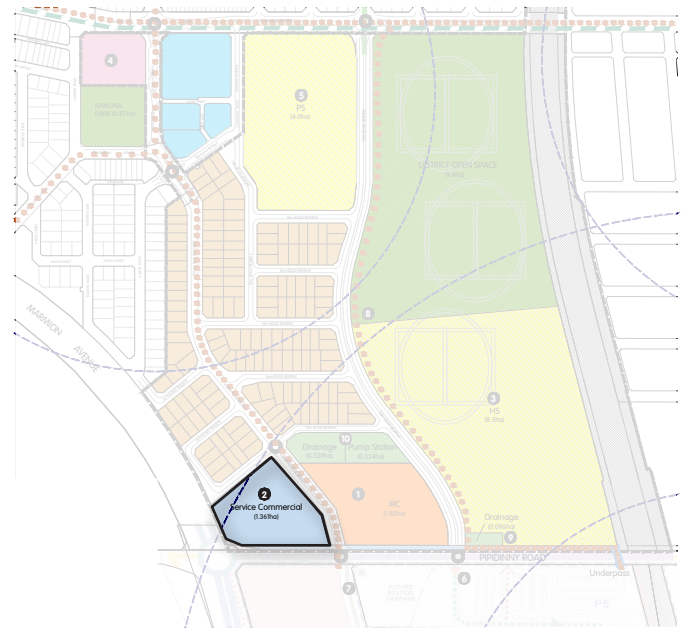
2.5. Modifications to Residential Zone Adjacent Pipidiny Road

As envisaged during the original structure planning, Pipidiny Road was to be a medium-high density residential precinct, located within and adjacent to the 400 metre walkable catchment of the District Centre and Railway Station. As development has progressed within the station precinct, the development response to Pipidiny Road has changed significantly, with the majority of the eastern extent west of the railway line now constructed as PTA carparks for railway users. This makes the conditions for residential development on the adjacent side of the road in Allara significantly less desirable.

The redistribution of the various institutional uses within Allara Estate adjacent Pipidiny Road provides an effective alternative to these changing conditions, with the IRC, High School and proposed Service Commercial precinct all benefiting from direct access to Pipidiny Road. Residential uses are consolidated with the existing residential precinct south of the Village Centre and provided good physical separation from the higher traffic area on Pipidiny Road. Likely to be targeted at a more affordable price point, future residents are provided with extensive recreational, commercial and institutional offerings in this precinct as key infrastructure is brought on-line.

Residential Density within the amendment area is set as an extension of the current R25-R60 density of the adjacent residential precinct. Dwelling yield on a whole-of-estate basis continues to achieve the current dwelling target of 3,406 dwellings specified at Part One, Clause 2.1 of the Structure Plan.

While the current amendment does not propose any change to the remaining R60-R100 area east of the railway line, this may also need to be reviewed at the appropriate time to consider relevant external factors.



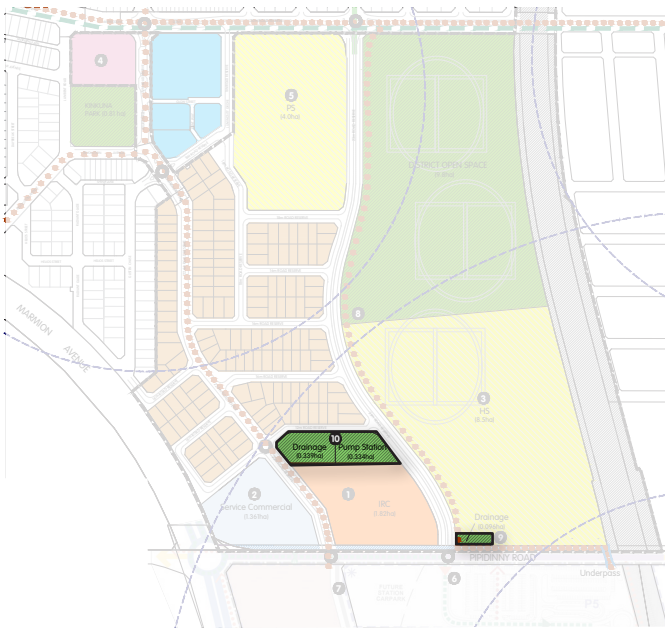
2.6. Inclusion of Service Commercial Zone

Noting the modified physical context referred above, the amendment also proposes the introduction of a new 1.36ha Service Commercial zone at the north-eastern intersection of Marmion Avenue and Pipidiny Road. The current zoning and design depicted in the Structure Plan for the area is Residential R25-60 with an area of open space and drainage taking advantage of the levels in this location.

The Economic Assessment prepared in support of the Amendment confirms that considering changes in the local planning context, the highest and best use for the land located along the northern side of this portion of Pipidiny Road is now Service Commercial rather than residential.

The site is well suited to this commercial use, considering the direct link to Marmion Avenue, and the adjacent railway station, station carpark and District Centre, will produce significant volumes of vehicle traffic creating strong opportunities for car-based commercial services. The Economic Assessment confirms that a Nett Lettable Area of 7,000sqm is supportable on the site.

Retail based uses, such as supermarkets were not considered appropriate considering the retail offer within the Eglinton District Centre to the immediate south which has the potential to accommodate multiple full-line supermarkets by 2030.



2.7. Public Open Space

In addition to the District Open Space which remains unchanged in area, the revised design includes two new areas of open space required for drainage and servicing.

The revised POS Area 6 and 6(a) comprise a nett area of 0.67 hectares comprising the required wastewater pump station and drainage to service this portion of the estate, which was previously located in the south-western corner and now occupied by the proposed Service Commercial zone. The revised location on the northern boundary of the IRC enables this landscaped zone to act as a visual buffer for the houses immediately to the north, screening the anticipated parking and service areas of the IRC from the view of residents.

The new POS Area 6(b) comprises a 960 sqm area integrated with the High School to accommodate stormwater drainage from Pipidinny Road, which was not provided for in adjacent development.

The POS Schedule for the Structure Plan has been updated accordingly to reflect the modifications to the design referred above and is included below.

Table 1 - Public Open Space Schedule (December 2023)

Site Area						232.43ha
Deductions						
Dedicated drainage				0.85 ha		
Reserve for Water and Sewerage (6a)				0.33 ha		
Reserve for Water and Sewerage (12c)				0.44 ha		
Reserve for Drainage (PTA) (18)				0.32 ha		
Reserve for Drainage (PTA) (19)				0.76 ha		
Temporary Pump Station				0.12 ha		
Neighbourhood Centre				2.04 ha		
Mixed Use Sites (50%)				0.45 ha		
Service Commercial				1.36 ha		
High School				8.50 ha		
Primary Schools				8.00 ha		
Railway Reserve (additional land-take to existing reserve)				2.23 ha		
Pippidiny Road Widening (7 metres)				0.37 ha		
					25.77 ha	
Gross subdivisible area						206.66 ha
Public open space @ 10 per cent						20.67 ha
Public open space contribution						
May comprise:						
-minimum 80% unrestricted POS					16.53 ha	
-maximum 20% restricted use POS					4.14 ha	
Public open space contribution	Open Space Area (Nett)	Unrestricted	Unrestricted - Conservation	Restricted - Drainage (>1:1)	Drainage 1:1 (not included in POS)	Status
1	0.50 ha	0.30 ha		0.20 ha		Future
2	0.50 ha	0.35 ha		0.15 ha		Future
3	7.42 ha	2.05 ha	5.16 ha	0.21 ha		Future
4¹	1.31 ha	0.33 ha		0.98 ha		Future
6	0.34 ha				0.34 ha	Future
6b	0.10 ha				0.10 ha	Future
7	9.80 ha	9.80 ha				DCP Item
8	0.53 ha	0.35 ha		0.07 ha	0.11 ha	Complete
9	1.59 ha	1.51 ha		0.04 ha	0.04 ha	Complete
10	0.68 ha	0.55 ha		0.08 ha	0.05 ha	Under construction
11	1.56 ha	1.11 ha		0.27 ha	0.18 ha	Under construction
12	1.48 ha	0.47 ha	0.81 ha	0.20 ha		Under construction
12a	0.79 ha	0.79 ha				Under construction
12b	0.59 ha	0.59 ha				Complete
13	1.75 ha	1.13 ha	0.53 ha	0.06 ha	0.03 ha	Under construction
14	0.31 ha	0.31 ha				Complete
16²	0.81 ha	0.81 ha				Complete
17	0.21 ha	0.21 ha				Future
Community Purposes Site (Indoor Recreation)	1.82 ha	1.82 ha				DCP Item
	32.09 ha	22.48 ha	6.50 ha	2.26 ha	0.85 ha	
Sub-Total		Unrestricted = 28.98 ha		Restricted = 2.26 ha	Deduction = 0.85 ha	
Total Public Open Space Provision		31.24 ha = 15.12%				

¹ (Consolidated with former POS 5)² (Consolidated with former POS 15)



Figure 2 - Public Open Space Plan

2.8. Other Modifications

1. Modification of Railway Reserve

The Amendment includes updated cadastral information associated with widening of the railway reserve for the Yanchep Rail Extension. This has been necessitated by changes to the railway reserve requested by the PTA, comprising widening along the length of the reserve to both the east and west, as well as the inclusion of additional drainage areas

The modifications result in a slight reduction of developable area within the Estate, which has been accommodated within the relocation and reconfiguration of the High School and District Open Space areas.

2. Pipidinny Road Widening

The Amendment identifies 7 metres of potential road widening within the JV land along the northern boundary of Pipidinny Road, to accommodate the future operational requirements of the road based on modelled volumes. This has been requested by DPLH, noting that adjoining landowners have already commenced development without providing for the necessary widening.

If the allocated road widening is determined not to be required as part of the of the assessment i.e. on the basis of road widening already provided by others, or reasonable reductions to the required cross section (such as removal of on-street parking) then this aspect of the proposal should be modified accordingly.

3. WAPC Planning Manual: Guidance for Structure Plans (August 2023)

The Amendment provides an opportunity to undertake minor amendments to relevant sections of the Structure Plan Report where this is considered appropriate for the effective operation of the Structure Plan. The proposed changes result in a greater alignment with the format and terminology set out in the WAPC's "WA Planning Manual – Guidance for Structure Plans", however as a legacy Structure Plan that is well progressed, it is not considered necessary to ensure total alignment with the WAPC Guidance for the continued implementation of the development.

The range of anticipated report modifications is summarised in the Schedule of Modifications at the beginning of this report. Considering the potential scope of changes proposed, the specific detail of these modifications is proposed to be agreed once the major elements of the Structure Plan are resolved as part of the City of Wanneroo / DPLH assessment.

3.0 Summary of Agency Engagement

The applicants have undertaken extensive agency liaison during the preparation of the Structure Plan amendment, investigating and refining a variety of design options to suit the dynamic planning environment.

The summary below highlights key meetings undertaken during preparation of the Amendment, not including various informal telephone and email communications.

Stakeholder	Date	Comment
City of Wanneroo	3 rd September 2020	<ul style="list-style-type: none"> Initial meeting to discuss relocation of IRC with a larger site closer to Railway Station. City undertaking Needs and Feasibility study to review the size, function, staging and design of the facility. Noted need for Dept Education involvement.
City of Wanneroo	21 st October 2020	<ul style="list-style-type: none"> City clarified operational requirements for IRC arising from its review, and those for the DOS. City noted preference for DOS in north/south alignment and schools within walkable catchment of district or neighbourhood centres.
Department of Education	February – June 2021	Ongoing mail correspondence with the Dept to share and review the emerging design concepts for the primary and high school locations and configuration, and in-principle agreement to a preferred option which generally aligned with the current SP Amendment design.
City of Wanneroo / DPLH	14 th February 2022	Presented proposed SP redesign incl. separation of high school and primary school per Dept Education request, road connections aligning with proposed Eglinton District Centre ACP and service commercial on Pipidiny Rd.
City of Wanneroo / DPLH / Dept Education	18 th October 2022	<ul style="list-style-type: none"> Agency workshop to review updated design concept. City was happy with revised IRC location. Dept Education was happy with separation of schools. Applicant proposed second north / south neighbourhood connector linking to Impressions Drive and relocation of PSP to this road.
City of Wanneroo	6 th April 2023	Design review with City. City generally happy with the location and mix of land-uses and noted support for new north / south neighbourhood connector with relocated PSP.

4.0 Technical Information

4.1. Bushfire Management Plan

The Amendment area is predominantly designated bushfire prone on the Map of Bush Fire Prone Areas therefore, bushfire risk considerations and assessment are required to inform the proposal per requirements under Policy Measure 6.2 of State Planning Policy 3.7 Planning in Bushfire Prone Areas (SPP3.7; WAPC 2015).

The Amendment is therefore supported by an addendum to the original Allara Estate Structure Plan FMP (FirePlan WA 2013) and subsequent addendum to support a subdivision of stages 10-22 (Strategen 2018) and other stages. The BMP addendum should be read in conjunction with this previous reporting.

The Addendum includes a revised assessment against the bushfire protection criteria of Guidelines for Planning in Bushfire-Prone Areas Version 1.4 (the Guidelines; WAPC 2021) and demonstrates that compliance can be achieved at subsequent planning stages.

The post-development BHL assessment identifies that all future habitable development will be located on land with either a Low or Moderate BHL.

The Assessment concludes that the bushfire hazards within and adjacent to the project area and the associated bushfire risks are readily manageable through standard acceptable solutions management responses outlined in the Guidelines. JBS&G considers that on implementation of the proposed management measures, the project area will be able to be developed with a manageable level of bushfire risk whilst maintaining full compliance with SPP3.7 and the Guidelines.

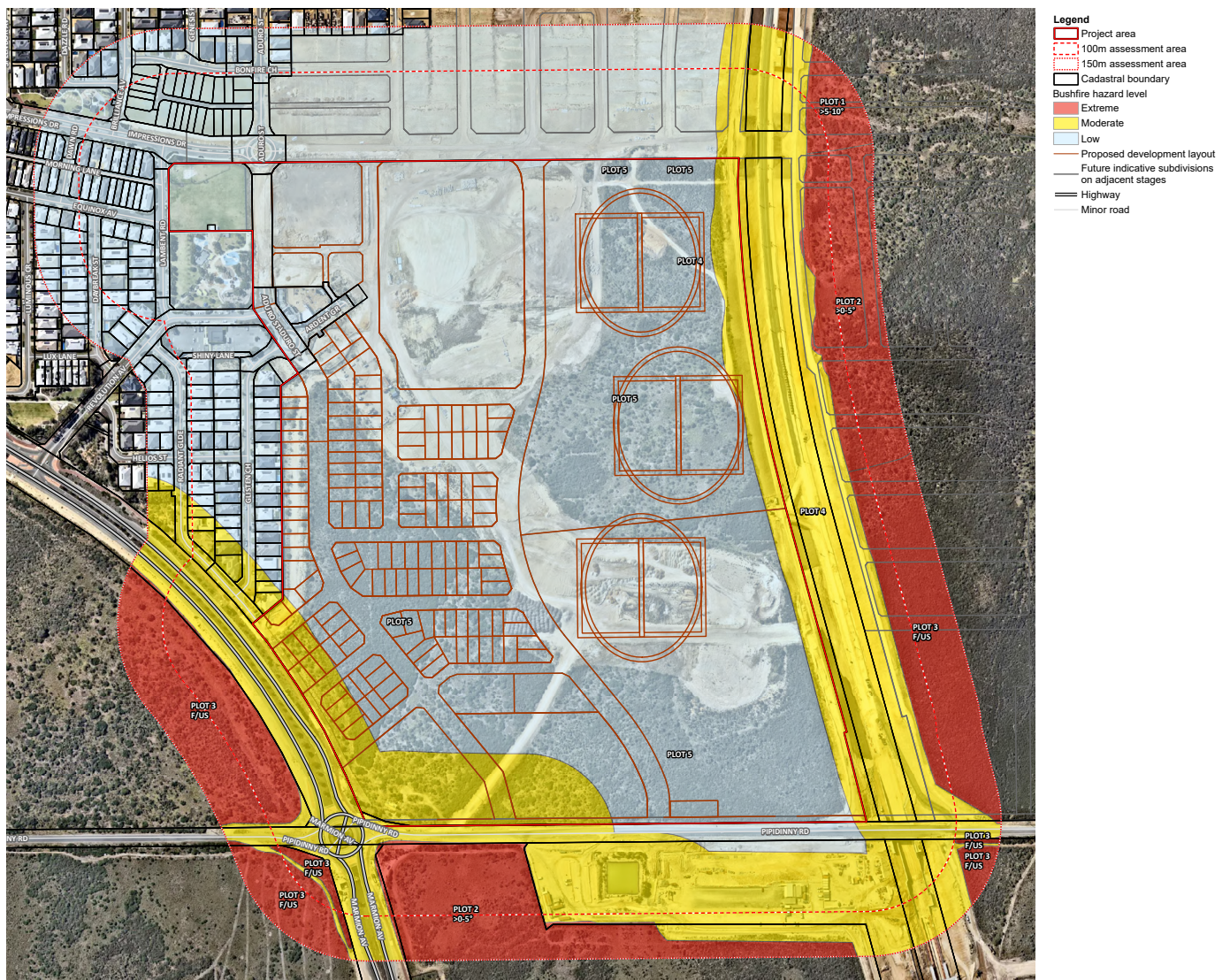


Figure 3 - Post Development Hazard Levels (JBS&G)

4.2. Traffic

The SP Amendment is supported by an updated Transport Impact Assessment report which replaces the existing North Eglinton Local Structure Plan Transport Assessment (December 2013). The analysis finds that the proposed modifications depicted in the Amendment result in an improvement in traffic movement throughout the Estate.

Based on the target dwelling yield of 3,406 dwellings, the Estate is anticipated to generate traffic flows of approximately 28,220 vehicles per day, with approximately 6,760 vpd of those being trips to and from the adjacent Eglinton District Centre Activity Centre Structure Plan area south of Pipidiny Road.

The road network of the LSP area has been designed based on WAPC Liveable Neighbourhoods guidelines to accommodate the future traffic flows that will be generated in this area.

The proposed LSP Amendment results in two north-south neighbourhood connector roads between Pipidiny Road and Impressions Drive in the southwest quadrant of the LSP area for access to and connectivity between the two schools, neighbourhood activity centre, indoor recreation centre and commercial development site.

Key intersections of the structure plan have been analysed for future traffic flows when the Eglinton area is fully developed, and the analysis indicates that all three intersections will operate satisfactorily during weekday AM and PM peak periods.

The proposed road network will accommodate a planned future bus route through the LSP area, which will provide satisfactory public transport access in future. The proposed LSP also provides for a comprehensive network of shared paths and footpaths to encourage and facilitate non-motorised travel as well.

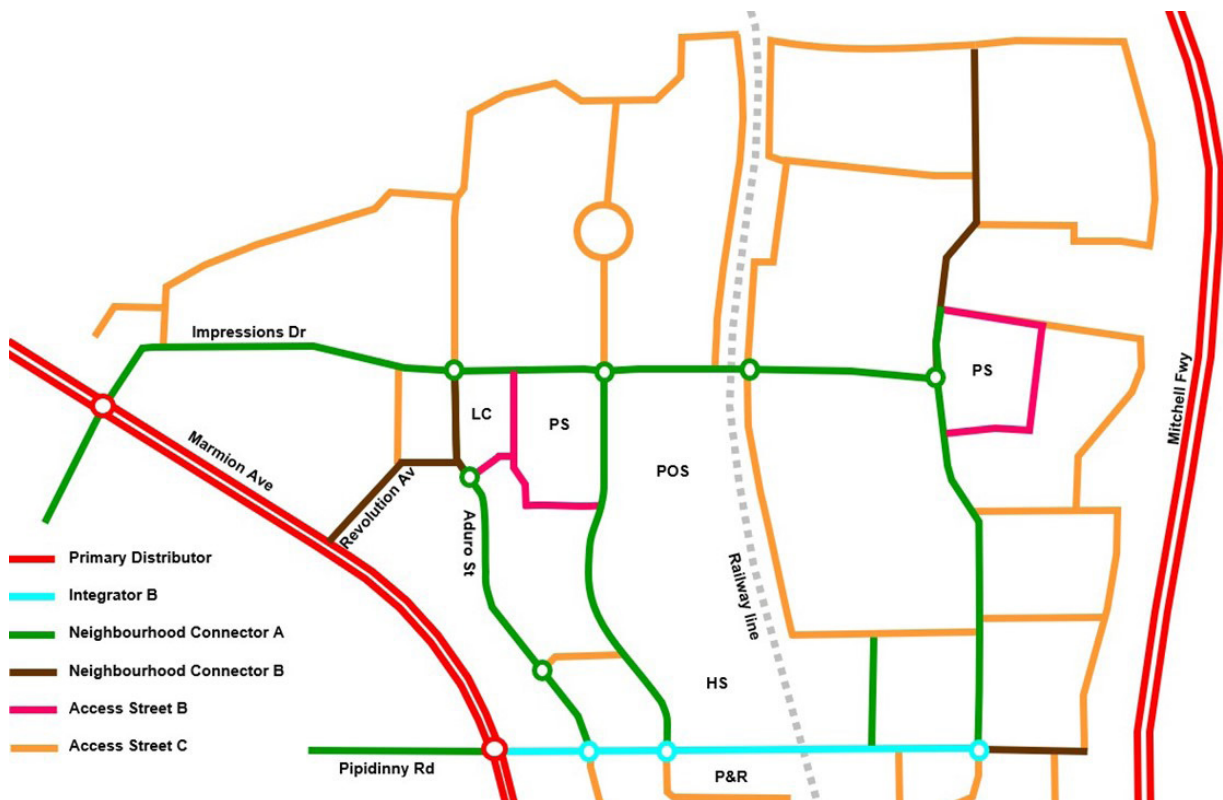


Figure 4 - Road Hierarchy (Transcore)

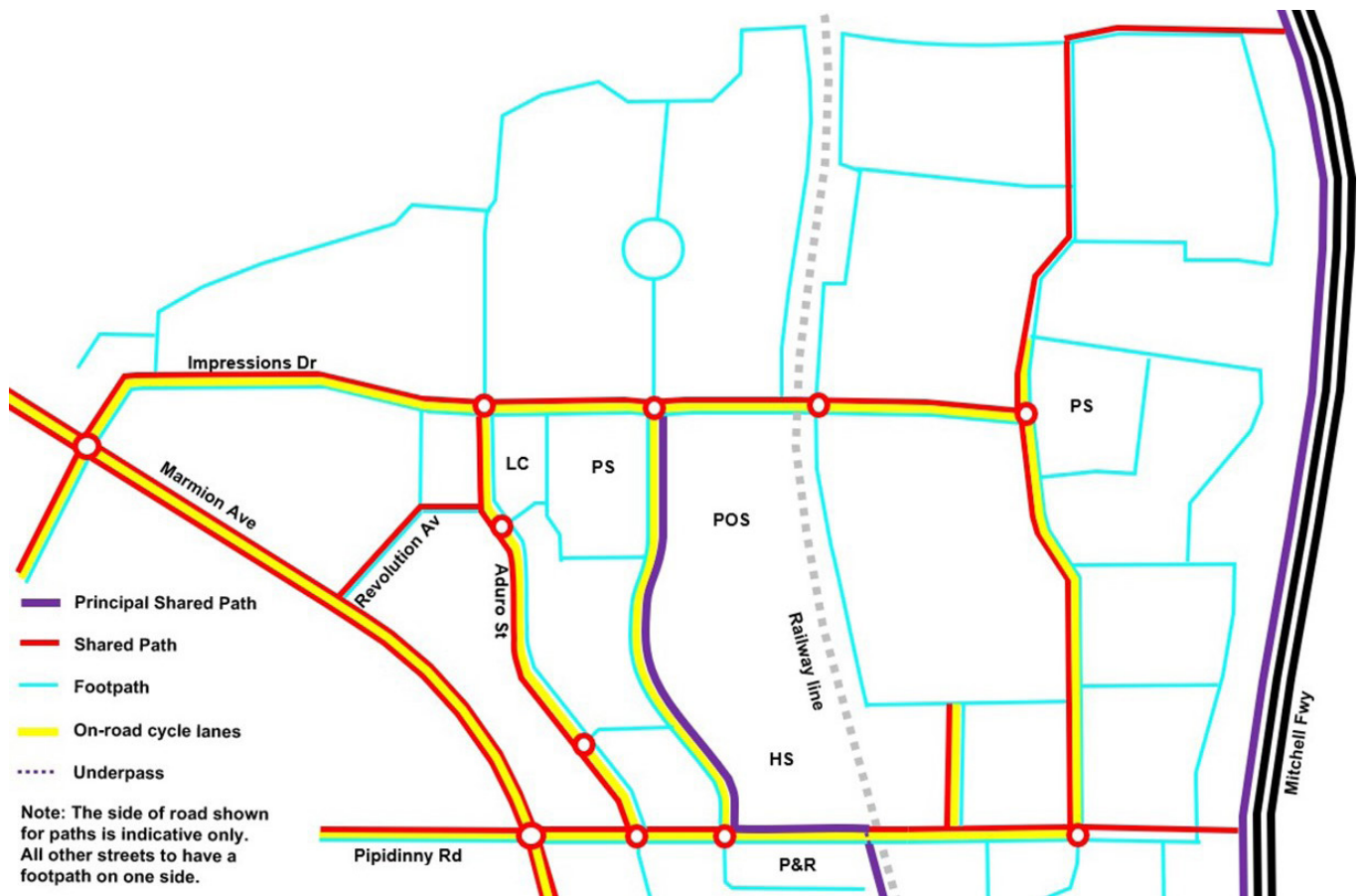


Figure 5 - Pedestrian and cyclist facilities (Transcore)



Figure 6 - Future daily traffic volumes (Transcore)

4.3. Local Water Management

Development of the Allara Estate is undertaken in accordance with a Local Water Management Strategy (LWMS) prepared by Emerge Associates in 2013 and approved by the Department of Water and Environmental Regulation (DWER) and the City of Wanneroo.

The purpose of the LWMS Addendum is to summarise the implications to water management across the site resulting from the proposed LSP amendment, and to demonstrate that the water management criteria approved in the LWMS can still be achieved. This addendum should be read in conjunction with the approved LWMS.

The principle behind the stormwater management strategy for Allara is to maintain the existing hydrology by retaining surface flows and to infiltrate the stormwater runoff as close to source as possible. The Allara LSP area will retain all runoff up to the 1% AEP event within the site. The small rainfall event runoff (the first 15 mm) will be retained as close to source as possible using a combination of soakwells, lot permeable areas, verge swales and bio-retention areas (BRAs) within POS. Runoff from events greater than the small rainfall event (the first 15 mm) will be conveyed downstream via surface flow and the pipe network to flood storage areas (FSAs) within POS where it will infiltrate to groundwater.

Surface water runoff modelling completed to support the original LSP has been updated to incorporate the proposed amendments and demonstrates that the revised LSP can still meet the requirements of the stormwater management strategy for the site.

Specific conclusions and recommendations include:

1. Residential lot drainage: Revised lot drainage assumptions based on the City's requirements that there should be no runoff from private lots; therefore, all residential lots will retain and infiltrate rainfall up to the 1% AEP.
2. Non-residential lot drainage: Retention of the requirement that all runoff up to the 1% AEP will be required to be retained within non-residential lots (commercial/business, school and district open space).
3. Accommodation of stormwater for the widening of Pipidiny Road. Allowance is made for 50 % of the road reserve in the catchment analysis, with the required drainage shown adjacent the High School at the catchment low point.
4. An additional 42m² of stormwater (1 % AEP event) from Ct-11.2a is proposed to be directed into the pit and pipe network of and ultimately into BRA 11/FSA 11. The capacity of BRA11 and FSA 11 will need to be revised to ensure that the additional 41m³ can be accommodated.
5. An additional 142m² of stormwater (1 % AEP event) from Ct-12a is proposed to connect into the pit and pipe network and ultimately into FSA 12C. The capacity of FSA 12 will need to be revised to ensure that the additional 179 m³ can be accommodated.
6. The ultimate location and configuration of these features will be confirmed at detailed design stage and presented in the relevant Urban Water Management Plans.

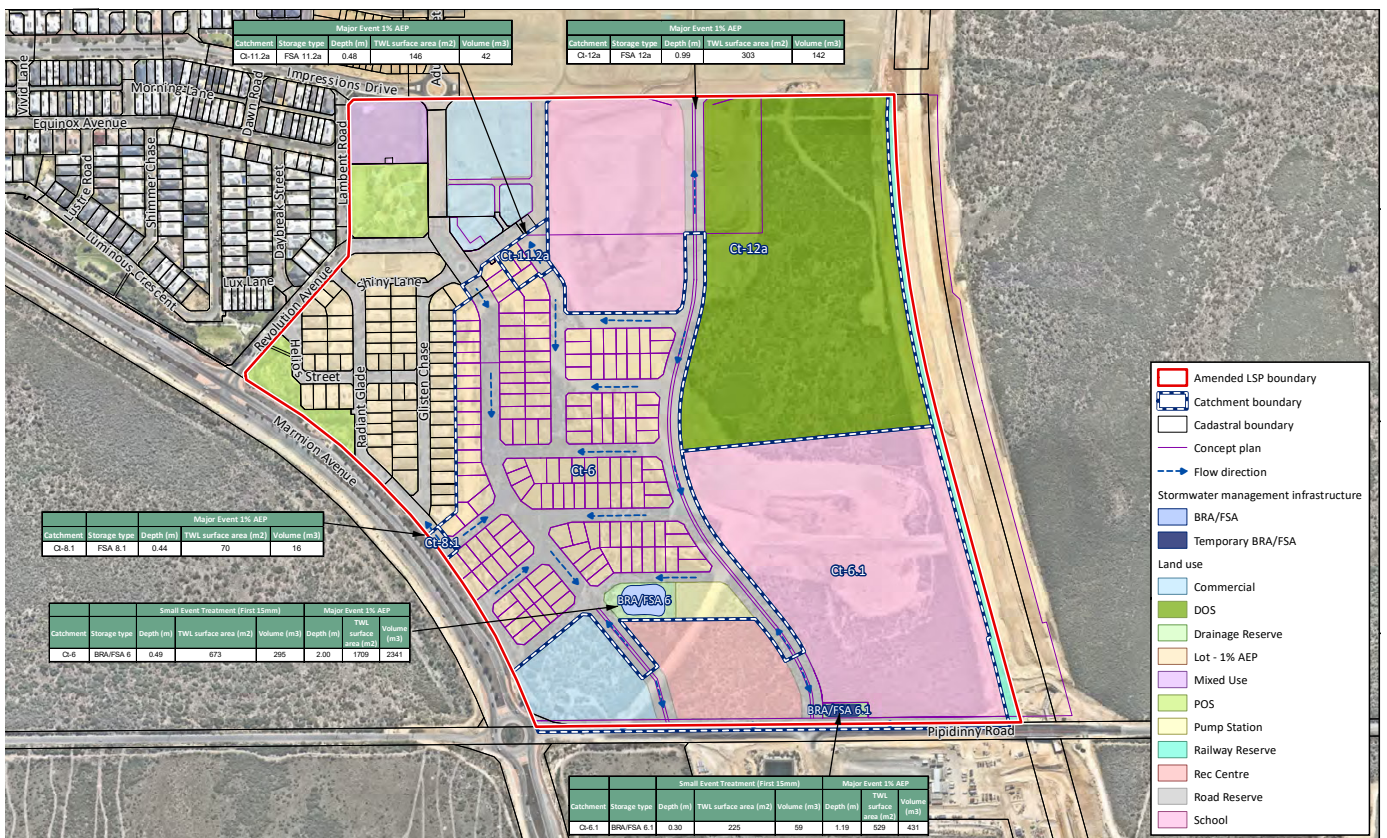


Figure 7 - Stormwater Management Strategy (Emerge Associates)

4.4. Acoustic Assessment

Marmion Avenue and the Yanchep Rail Extension are designated as 'other significant freight/traffic routes' under the Western Australian Planning Commission (WAPC) Planning Policy 5.4 'Road and Rail Noise' (SPP 5.4). As portions of the Structure Plan Amendment fall within the trigger distances of this infrastructure, an Acoustic Assessment has been undertaken, specifically looking at the potential noise impacts of the modified railway alignment on the adjacent playing fields and the High School. This supplements the acoustic advice provided in Herring Storer Acoustic's 2013 approved report (Ref 14841-5-12111) which has guided all development within the Estate.

With respect to the Yanchep Rail Extension, the PTA has provided detailed design for the rail line, detailing the cutting and topography in a 3D representation. The modelled noise contour plot for the area adjacent the passenger rail line shows noise levels below the LAeqday 55 dB(A) noise criteria, resulting in no noise requirements for adjacent development.

This also applies to development on the relocated High School site meaning there are no spatial restrictions or building controls on the future development of this facility.

With respect to Marmion Avenue, the outcomes of the existing Acoustic Assessment continue to apply, which identify the need for quiet house design package requirements for lots that are impacted by noise from Marmion Avenue. This would be implemented as conditions of subdivision approval for the affected stages.

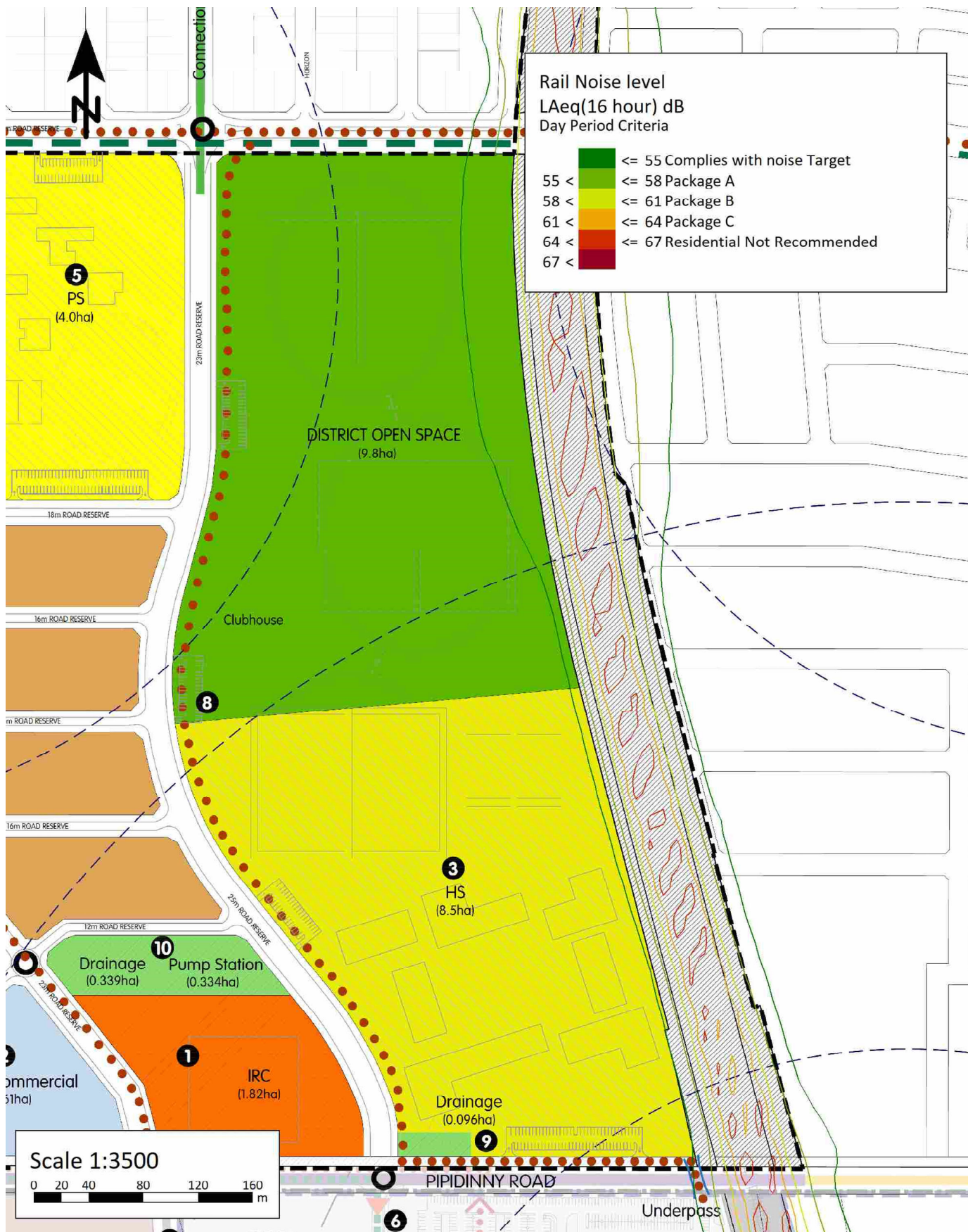


Figure 8 - Noise Contour Plot (Herring Storer Acoustics)

4.5. Services Report

To support the Amendment request Stantec has provided an addendum to the original 2012 Engineering Services Report, which identifies the following updates to the servicing requirements for the Estate as a result of the proposed structure plan modifications.

Earthworks

The earthworks design parameters are as per the original report, however the concept earthworks plan has been amended to suit the new design.

The PTA are completing minor upgrades to Pipidiny Road as part of the Eglinton Train Station Development Project. The upgrades to be completed by the PTA exclude any consideration of regional levels for adjacent developments. Pipidiny Road surface levels remain at existing, which is incorporated into the earthworks strategy.

As a result of the Pipidiny Road works, the structure plan is unable to achieve cut to fill balance within the project area without compromising on the proposed levels of institutional facilities. Exportation of fill will be required to achieve levels shown on the plan.

Stormwater

The overall stormwater strategy remains per the original report. Design principles are consistent with the Local Water Management Strategy (by Emerge Associates). Stormwater drainage volume calculations will include runoff from the northern half of Pipidiny Road that will need to be gravitated to infiltration basins within the project area boundary.

The proposed basin location near the proposed indoor recreation centre site will provide storage for stormwater flows. The potential basin included adjacent the high school site is sized to cater for the Pipidiny Road stormwater runoff (north of centreline), noting that the PTA Eglinton Train Station Development Project only accounts for Pipidiny Road runoff (south of centreline) in its drainage design and calculations.

Sewer

The southern catchment will gravitate to a pump station and possibly be ultimately pumped to the future Yanchep Branch Sewer. Timing for Yanchep Branch Sewer remains subject to regional land development. In the interim, the pump station will temporarily discharge into the 450 HDPE sewer main on Marmion Avenue, (pump station location indicated in Figure A, PM route indicated in Figure B). Wastewater planning should be reviewed by the Water Corporation to confirm this remains the optimal approach for discharging wastewater flows.

Water, Power, Communications and Gas

The supply strategy for these utilities will not change from the original report.

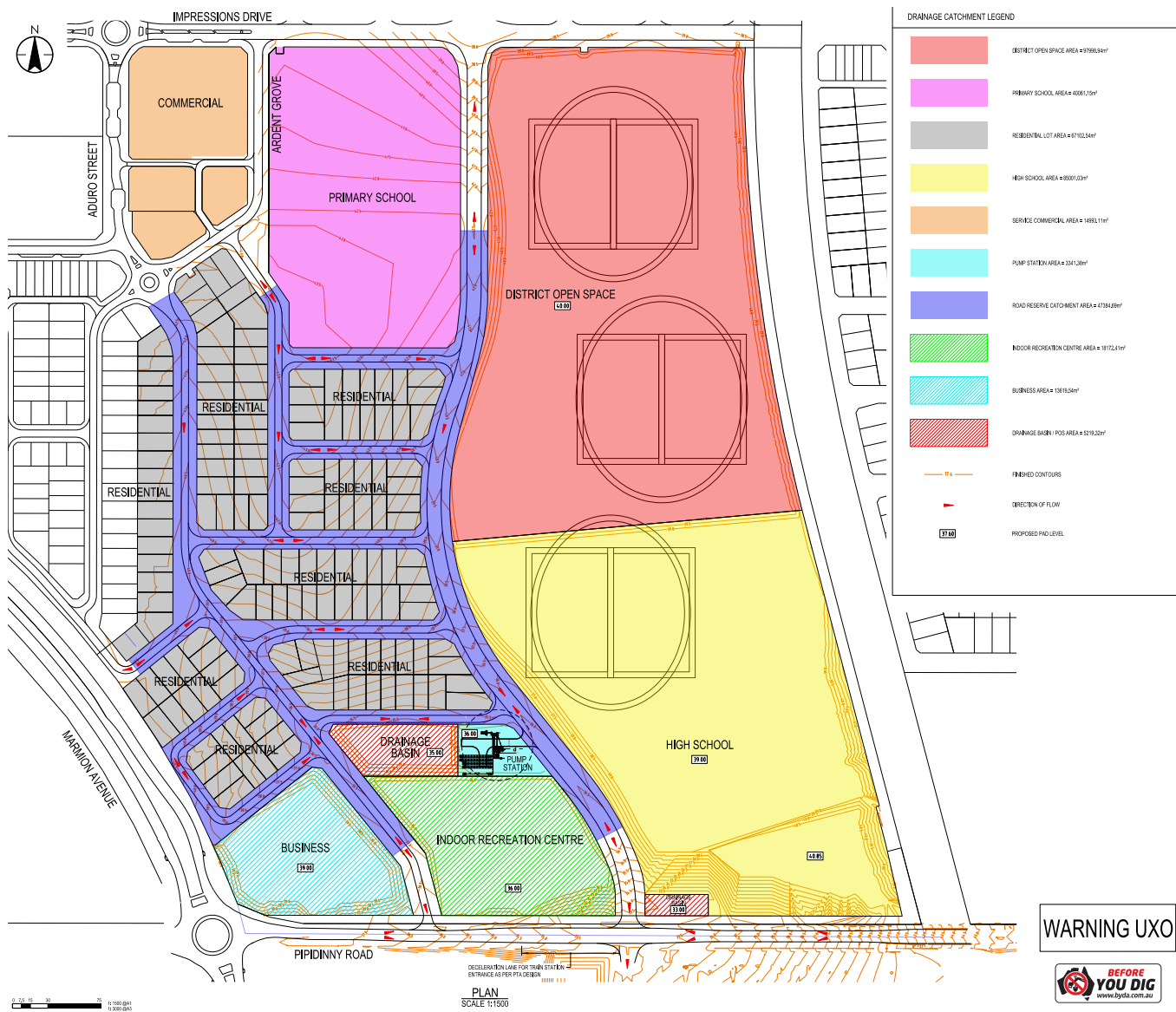


Figure 9 - Stormwater Catchment Plan (Stantec)

5.0 Conclusion

The proposed Amendment is the culmination of many years of design investigation and negotiation with relevant agencies and the project team to optimise the location of institutional infrastructure at the Allara Estate. This has occurred within a dynamic economic, planning and approvals context, with relevant changes being accommodated within the evolving design response as these matters have arisen.

The Amendment:

- Relocates and reconfigures the future Indoor Recreation Centre and High School to improve their proximity to the Eglinton Railway Station.
- Separates the Primary School and High School to better reflect the Department of Education's current operational requirements.
- Updates the configuration of the railway reserve for the Yanchep Rail Extension to align with land agreements between the PTA and DevelopmentWA.
- Introduces a modestly sized Service Commercial zone on Pipidinny Road in response to the changing land-use conditions resulting from changes to the Eglinton District Centre area and the delivery of works associated with the new Eglinton Railway Station.
- Consolidates and centralises the proposed residential area away from Pipidinny Rd to facilitate more effective staging and delivery to meet the high demand for housing.

The proposed changes have been extensively socialised and tested with relevant agencies resulting in a considered, long-term plan to guide future development of the Estate.



Appendix 1 - Economic Assessment (Hatch)

Appendix 2 - Bushfire Management Plan Addendum (JBS&G)

Appendix 3 - Transport Impact Assessment (Transcore)

Appendix 4 - Local Water Management Strategy Addendum (Emerge Associates)

Appendix 5 - Acoustic Assessment – Passenger Rail (Herring Storer Acoustics)

Appendix 6 - Engineering Services Report Addendum (Stantec)

